

DESIGN STATEMENT

Proposed Development : **GA2; Residential Development
Baldoye, Co. Dublin**



Architects, Interior Designers, Master Planners



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1.0 Introduction

This Design Statement has been prepared by CCH Architects, on behalf of our Client, Lismore Homes Limited, to accompany a Strategic Housing Development (SHD) application for a proposed residential development at Baldoyle-Stapolin, Dublin 13

The proposed development will consist of the construction of 1,007 apartments, 6 no. communal residential community rooms, and a ground floor crèche in 16 no. buildings with heights varying from 4 to 12 storeys, basement and surface level car parking, secure bicycle parking, landscaping, water supply connection at Red Arches Road, and all ancillary site development works on a c. 6.1 hectare site.

The proposed development of 1,007 no. apartment units comprise the following types:

- 58no. Studio Apartments
- 247no 1-Bed Apartments
- 94no 2-Bed 3 Person Apartments
- 563no 2-Bed 4 Person Apartments
- 45no 3-Bed Apartments

The proposal also includes an 800 sq.m crèche with an outdoor play area of 208 sq.m. and 6 no. communal residential community rooms.

The Project Design Team members are as follows:

- Architects: **CCH Architects**
- Civil & Structural Engineers: **J.B. Barry and Partners Ltd.**
- Planning Consultants: **Kieran O'Malley & Co Ltd**
- Landscape Architects: **Murray & Associates**
- Daylighting & Wind Analysis: **BRE Group**
- Environment Impact Assessment Consultants: **AWN Consulting Engineers**
- Ecologist: **Altamar Marine & Environmental Consultancy**
- Site Lighting Design: **Ethos Consulting**
- Building Lifecycle Report: **Aramark Property**
- 3D Photomontage: **Modelworks**

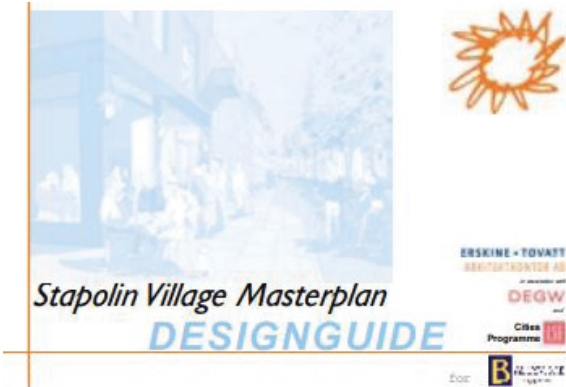
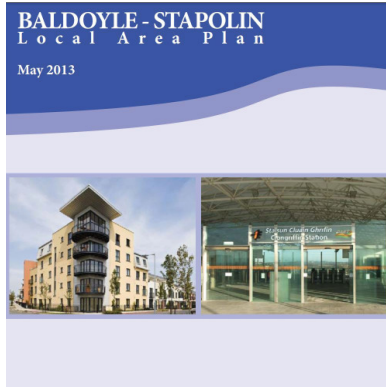
2.0 Statutory Planning Guidance

The proposed development has been designed having regard to the following national, regional and local planning documents:

- Project Ireland 2040 – National Planning Framework.
- Eastern & Midland Regional Assembly Regional Spatial & Economic Strategy 2019 – 2031.
- Fingal County Development Plan 2017 - 2023
- Baldoyle Stapolin Local Area Plan 2013 (as extended)



Fig 1; Aerial view of Baldoyle indicating site boundary (red)



- Sustainable Urban Housing: Design Standards for New Apartments (2020).
- Guidelines for Planning Authorities on Sustainable Residential Development in Urban Areas (2009)
- Urban Design Manual; A Best Practice Guide 2009
- Quality Housing for Sustainable Communities (2007)
- The Planning System and Flood Risk Management (2009).

2.1 Fingal Development Plan 2017 - 2023

The application site is zoned RA to Provide for new residential communities subject to the provision of the necessary social and physical infrastructure in the Fingal Development Plan 2017 – 2023.

The site and adjoining residential zoned lands are subject to LAP 10.A which is to Prepare and/or implement a Local Area Plan for lands at Baldoyle / Stapolin to provide for the strategic development of the area as a planned sustainable mixed use residential development subject to the delivery of the necessary infrastructure. The approved local area plan for the site is the Baldoyle Stapolin Local Area Plan 2013 (as extended) – the site is also identified for residential development in the LAP (fig.2).

There is also a specific local objective pertaining to the site; no. 95 which is to ensure that the visual impact of any development on the Greenbelt will be minimised by its siting, design and planting.

The LAP sets out four development themes that form the basis for its policies and objectives for the plan area. These four themes are:

1. Sustainable Development
2. High Quality Places for All
3. A New Heart for Baldoyle-Stapolin
4. Homes for the Future

By way of background to the LAP, the original Erskine & Tovate Masterplan for the Plan lands set out a clear framework for the development of the residentially zoned lands. It was envisaged that a new neighbourhood centre, located adjacent to the train station and based around a large public square, would provide the main central heart of the area incorporating retail, community and leisure facilities. It also set out a strong layout for residential areas primarily based on a block design with each block being developed around private courtyard areas. Higher density development was planned around 'Stapolin Square' and around the parkland edge to the north and east.

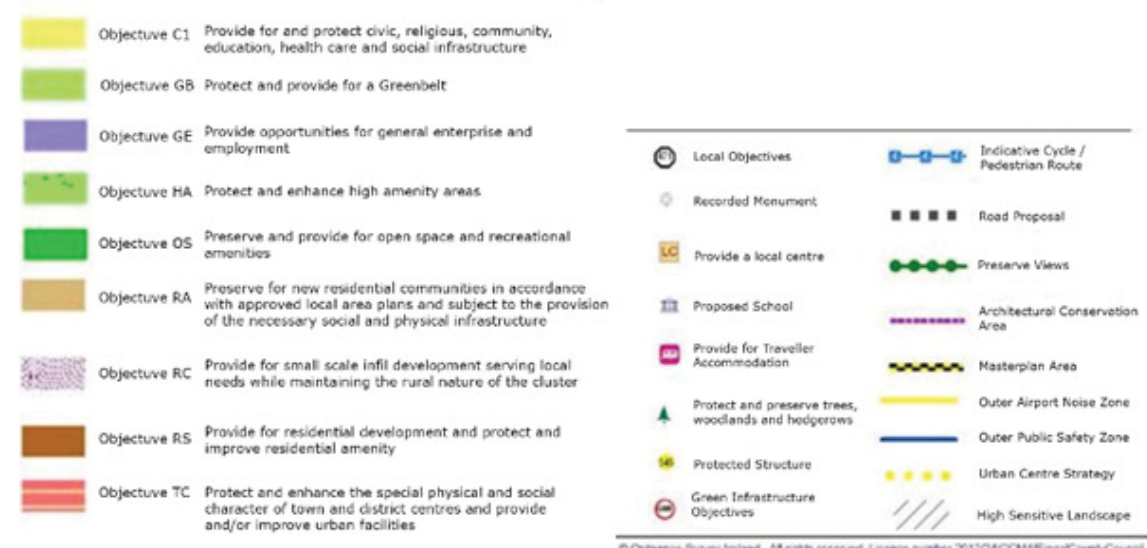
The LAP refined and updated the original Masterplan having regard to the prevailing planning policy and market conditions in 2013 and the incomplete residential development at Stapolin-Baldoyle. As the approved road network was already in place, the LAP continued with the overall pattern of residential development blocks but incorporated additional strategies in particular for green infrastructure, permeability, and sustainable transport.

The LAP created three growth areas to facilitate the planned and coordinated development of the remaining lands at Stapolin-Baldoyle. In doing so, it set out new provisions for higher density development and a building height strategy that focussed higher building at the planned local centre and along the development boundary to proposed Racecourse Park.



Fig 2; Site zoning map

Development Plan Extract



3.0 Design Context

3.1 Site Location

The site is situated in the northeast sector of the residential zoned land within the overall Coast residential development at Stapolin, Baldoyle. It borders the proposed Racecourse Park to the north and east, Stapolin Haggard to the south, and Growth Areas 1 and 3 to the south and west respectively. Clongriffin town centre is on the opposite (west) side of the Dublin to Belfast railway line and Clongriffin DART Station.

The site is divided into pre-determined sectors in accordance with the objectives of the local area plan. These are identified as Sectors 6A & 6B, Sector 7, Sector 8A, Sector 8B and Sector 8C. Baldoyle has a regular bus, DART, and rail connection to Dublin City Centre, and is within easy reach of the M50 and the M1-A1 Dublin-Belfast motorway. There are local shops, schools, sports facilities & a public park within walking distance. Once complete, the entire Baldoyle Stapolin area will be within walking distance of the completed Clongriffin DART station and Racecourse Park, with Baldoyle village a short walk away.

The immediate local area is coastal in nature, with significant open space bordering Baldoyle Bay. The expansion of the nearby Dublin urban centres over the last number of decades has given the area a significantly suburban feel but this is now being supplanted with higher density apartment developments forming new urban centres around the DART station at Clongriffin and the Quality Bus Corridor at Malahide Road.

There are a few completed residential developments within the Coast scheme. These are known as Myrtle and Red Arches. They comprise primarily houses, duplex units, and apartments in building ranging in height from 2 to 5 storeys (figs. 4-6). The general layout plan for these developments follows the building block pattern set out in the initial masterplan for the area.

There is also a large area of undeveloped land to the south and west of the application site. Lands to the south form part of Growth Area 1 and are currently under construction. Development has commenced on new housing in accordance with Reg. Ref.: F16A/0412. This is south and southwest of the site and forms part of GA1 per the LAP. There are undeveloped lands to the west and southwest which are GA3 and GA1 respectively. Planning permission has been granted for these lands and they are discussed in the following section.

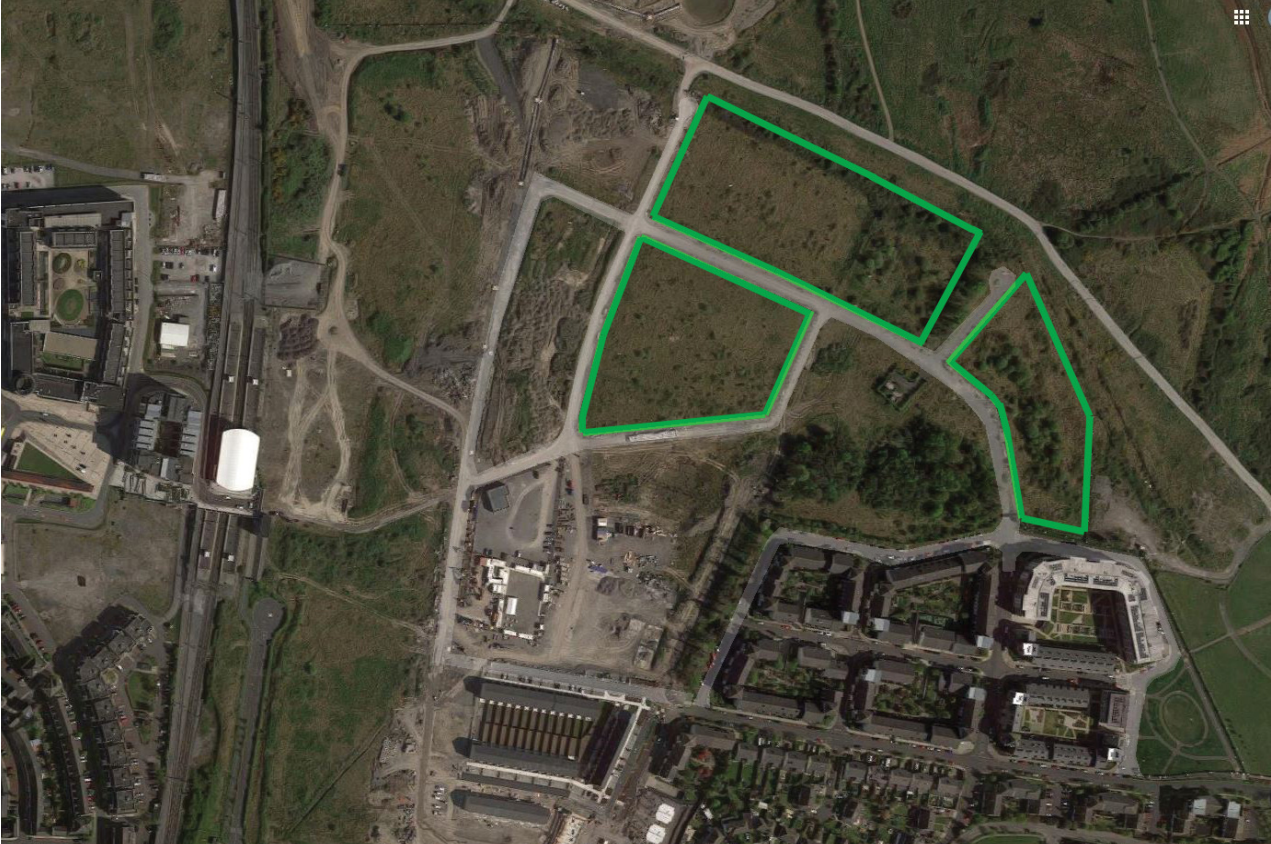


Fig 3; Aerial view of Baldoyle indicating site ownership boundaries (green) and existing road network



Fig 4; Existing site plan with site outline and site ownership boundaries



Fig 4



Fig 5



Fig 6

BALN5-CCH-00-ZZ-REP-A-240 P04 DESIGN STATEMENT INCLUDING 12 POINT DESIGN APPRAISAL



Fig 7; Baldoye Stapolin; Erskine & Tovatt Architects Masterplan

3.2 Extant Planning Permissions at the Coast

There is a current planning permission on the application site. Planning permission was granted by An Bord Pleanála (PL 06F.239732) to Regents Park Developments Limited for 400 no. dwellings units, 3 no. retail units, a crèche, etc. and ancillary site development works subject to planning conditions (fig.10). An extension of duration of that permission was issued by Fingal County Council by order dated 10th May 2018 to extend the duration of the planning permission up to 23rd May 2023.

There are also current planning permissions on the GA1 and GA3 lands (figs 8 & 9). A ten-year planning permission (Reg. Ref.: F16A/0412 and ABP 06F.248970) was granted by An Bord Pleanála for a proposed development comprising “550 number residential units (379 number apartments and 171 number houses) and a village centre comprising circa 1,585 square metres of commercial floorspace laid out in 13 number blocks (Blocks A1, A2, A3, B1, B2, B3, B4, C1, C2, C3, C4, C5 and D1) ranging in height from two-storeys to six-storeys” for part of the lands that comprise Growth Area 1. This approved development is currently under construction and it includes surface water drainage infrastructure that would also drain the subject site.

More recently, a strategic housing development application (ABP TA06F.310418) by Shoreline Partnership that sought amendments to approved plans Reg. Ref.: F16A/0412, F20A/0258 and F21A/0046 for the construction of for 882 no. residential units (135 no. houses and 747 no. apartments), creche and associated site works. The application site, which correlate to GA1 in the Baldoyle-Stapolin LAP, extended to 9.1 hectares and the scheme provided for 10 no. apartment blocks and 5 no. house groups ranging in height from 2 to 15 storeys. Planning permission was granted by An Bord Pleanála on 22nd September 2021 for the entire of the proposed development but that decision is now subject of a judicial review.

Planning reference ABP TA06F.311016 was a second strategic housing development application by Shoreline Partnership for the proposed development of the GA3 lands in the Baldoyle-Stapolin LAP. The proposed development was for 1,221 no. residential units in 9 no. apartment blocks with heights ranging from 2 to 15 storeys on a site of 6.89 hectares. Planning permission was granted by An Bord Pleanála on 23rd November 2021 for the entire of the proposed development but that decision is also now the subject of a judicial review.

There is a proposal for a new school to the south of the subject site. Reg. Ref. F19A/0461 & F21A/0498 was approved for a “Three storey 16 classroom Primary School building in Baldoyle (Roll Number 20519G), including a two classroom SEN base. The design also includes a general-purpose hall, support teaching spaces and ancillary accommodation, external junior play areas, secure SEN hard and soft play area and a sensory garden. The proposed project also incorporates associated car parking, access road, pedestrian access, bicycle lane, construction of 2 no. external ball courts, landscaping, connection to public services and all associated siteworks.” This site is situated at Grange Road at the southern end of the Coast development.



Fig 8; Site plan showing Growth Area 1 (Shoreline 1); Henry J Lyons Architects



Fig 9; Site plan showing Growth Area 3 (Shoreline 2); Henry J Lyons Architects



Fig 10; Existing permission on Growth Area 2; Project Architects



Fig 12; Site perspective, January 2021

3.3 Pre-planning enquiry and Stage 1 Submission to An Bord Pleanála; March 2021

The proposed development was submitted to Fingal County Council in April 2020 and November 2020 for consultation. Consultations took place with An Bord Pleanála in March 2021

Comments in the planners report which identified the key comments / concerns can be summarised under the following headings:

- Integration and quality of design
- Building Heights
- Architectural expression
- Character
- Materials
- Sense of Place
- Proposed Boundaries
- Proposed Entrances
- Usable roof spaces
- Residential Amenity
- Open Space and Proposed Landscaping

Fingal C/C indicated at the time that the scheme “lacked architectural merit” in its overall design. As the scheme originally presented consisted of a relatively uniform palette of materials and facade treatments across the whole scheme, the council also had concerns with regard to this approach, particularly in relation to what was regarded as the “monolithic treatment of the residential blocks”.

The council felt that the scheme presented an opportunity to create distinct character areas within the scheme. The various sectors presented the opportunity to create a “sense of place” within each of the sectors. Heights of the various blocks remained relatively uniform throughout the previous submitted proposal, with building heights ranging from 4-9 storeys. It was felt that heights could be refined to give greater prominence to certain vertical elements, creating a certain visual interest to the scheme.

The proposed elevation to Racecourse Park is seen as a key component within the development. The previously submitted proposal presented a relatively uniform facade with regard to scale, height and palette of materials which was seen as a key area to improve upon. The separation between these blocks along the Racecourse Park edge also presented the opportunity to emphasise both the visual and physical connection between the various courtyard gardens, and the racecourse park beyond. It was suggested that there was an opportunity to give greater expression to facade treatments, particularly with regard to the proposed palette of materials.



Fig 13; Racecourse Park perspective, January 2021



Fig 14; Stapolin Haggard Park perspective, January 2021



Fig 15; Site elevation along Stapolin Way, January 2021



Fig 16; Site elevation along Racecourse Park, March 2022



Fig 17; Aerial view of the proposed development looking north

4.0 12-Point Criteria for Sustainable Residential Development in Urban Areas

The primary design considerations, taking on board a site analysis, site and technical constraints are set out below and reflect the criteria set out in the 'Urban Design Manual – A Best Practice Guide 2009'.

4.1 Context

4.2 Connections

4.3 Inclusivity

4.4 Variety

4.5 Efficiency

4.6 Distinctiveness

4.7 Layout

4.8 Public Realm

4.9 Adaptability

4.10 Privacy and amenity

4.11 Parking and supervision of parking.

4.12 Detail Design

4.1 Context

The land is located within the Stapolin Baldoyle area. Baldoyle Racecourse Park is located to the north and east, with Stapolin Haggard located to the south. The entire site is within a 10 minute walk of Clongriffin DART station. The site is divided into pre-determined sectors in accordance with the objectives of the local area plan as well as the wider development objectives for Stapolin Baldoyle. These are indicated in our report as Sectors 6a&6b, Sector7, Sector 8a, Sector 8b and Sector 8c.

Baldoyle has a regular bus, dart and rail connection to Dublin City Centre, and is within easy reach of the M50 as well as the M1-A1 Dublin-Belfast motorway. There are local shops, schools, sports facilities & a public park within walking distance.

4.1.1 Developable Areas

The site consists of three primary blocks of land for residential development, the associated roadways between and adjoining those development blocks, and c. 170 metres of Red Arches Park to facilitate a watermain connection as requested by Irish Water. Excluding the lands within Red Arches Park, the site is effectively Growth Area 2 (GA2) per the Baldoyle Stapolin Local Area Plan 2013 (as extended).

The application site extends to c. 6.1 ha. of which the developable area is 4.59 ha. and made up of three distinct blocks of land. The site forms part of the overall Coast residential community that has been planned on c. 41 hectares of residential zoned land at Stapolin - Baldoyle, and around Clongriffin DART station with the c. 80 hectares Racecourse Regional Park to the north, northeast, and east of the overall Coast development.

Access to the site is currently available at Red Arches Park but following completion of the residential schemes for GA1 to the south and southwest, and GA3 to the west, additional access for pedestrians, cyclists and motorists will be available through those developments. The north and northeastern boundary of the site abuts Proposed Racecourse Park. On completion of that proposal, pedestrians and cyclist access will be available at the Park.

4.1.2 Aerial Photos

The aerial photographs (figs. 18 - 22) shows the full extent of the application site (in red), the three development blocks in green, and the approximate locations of the adjoining residential developments GA1 and GA3, and proposed Racecourse Park.



Fig 18; Aerial photograph showing the proposed site outline (red) and the developable areas (green)



Fig 19



Fig 20



Fig 21



Fig 22

4.1.3 Site Location

The site is within the Coast residential development at Stapolin-Baldoyle. It is due northwest of Baldoyle village, c. 6.5 km from Dublin Airport, and c. 10 km from Dublin City Centre. The site is within the administrative area of Fingal County Council but with the Coast development, it abuts the administrative area of Dublin City Council. The Coast is an emerging urban centre developed around Clongriffin Dart Station with a substantial local centre at Stapolin-Baldoyle in GA1 and generous recreational amenities through the provision of pitches at Coast Road, Mayne Road and the proposed Racecourse Park.

4.1.4 Site Analysis

The site comprises three blocks (sectors 6A/6B, 7 and 8) of residential zoned land with the sub-surfaces of the road network in place between these blocks. It is irregular shaped in plan encompassing two sides of Stapolin Haggard communal open space within the Coast and with c. 470 metres frontage to Proposed Racecourse Park.

The site slopes from south to north; this is more pronounced within sector 7. There are few natural features on the site except for a small stand of trees between sectors 8A and 8B. There is an existing cul-de-sac in the northeast section of the site that links the pumping station at Stapolin Haggard to the northern fringe sewer that is just beyond the site boundary.

Compliant with the development framework set out for the overall Coast development, there will be multiple access to the proposed development from the adjoining residential schemes. Approved and constructed developments abutting the site include building heights between five and ten storeys.

4.1.5 Development Strategy

The brief was to design a sustainable residential community to assimilate into the existing and emerging urban development at the Coast and in doing so, to provide an appropriate mix of units, density and building heights having regard to the latest planning guidelines.

Unlike a standard greenfield site, any development of this site must be planned within the established street and building block pattern that forms part of the overall Coast development. Having regard to the planning history of the site, the provisions of the local area plan and the various section 28 guidelines, the following planning objectives formed the basis for the development strategy:

- Compact urban design
- Foster community interaction
- Promote sustainable transport
- Achieve sustainable residential densities
- Encourage additional building height
- Plan an appropriate mix of unit types
- Deliver integration and connectivity with the Coast development

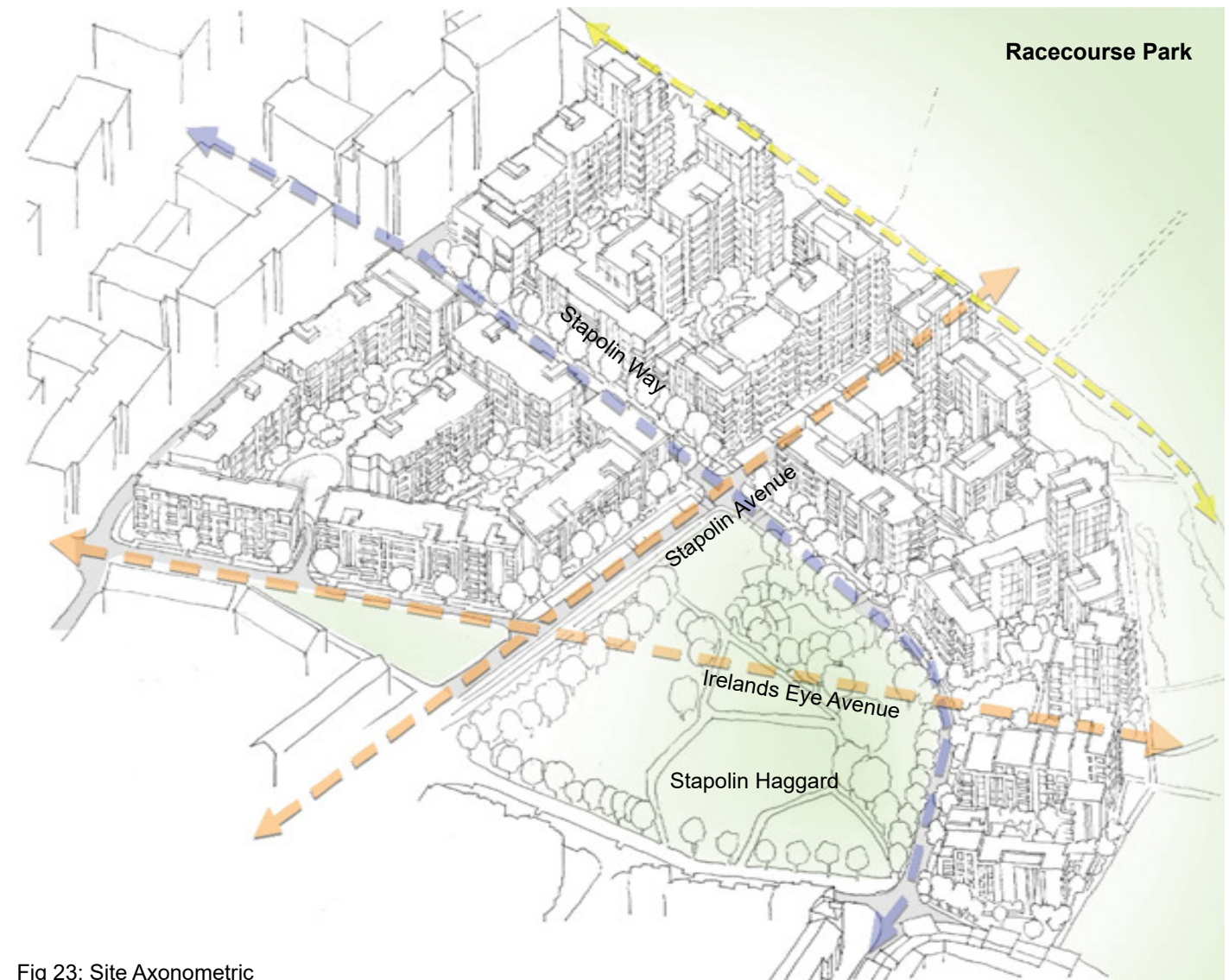


Fig 23; Site Axonometric

4.2 Connections

4.2.1 Transport and Local Connectivity

The site is very well served by existing and proposed high frequency and high capacity public transport including the existing Clongriffin Dart Station that is c. 240 metres from the site and the proposed BUS Connects that is proposed to serve the overall Coast development. In addition, the wider Stapolin-Baldoyle area has a footpath and cycle path network that links to the wider network between Howth, Dublin City Centre, and Dun Laoghaire (as part of the coast to coast cycle route).

The LAP and previous masterplan proposals for the overall Coast development have ensured that permeability and connectivity are core development themes that apply to the various phases of residential development at Stapolin-Baldoyle. This proposal will be fully integrated within the wider Coast development with direct pedestrian and cyclist linkages to the proposed local centre, open spaces, and Racecourse Park (fig.24).

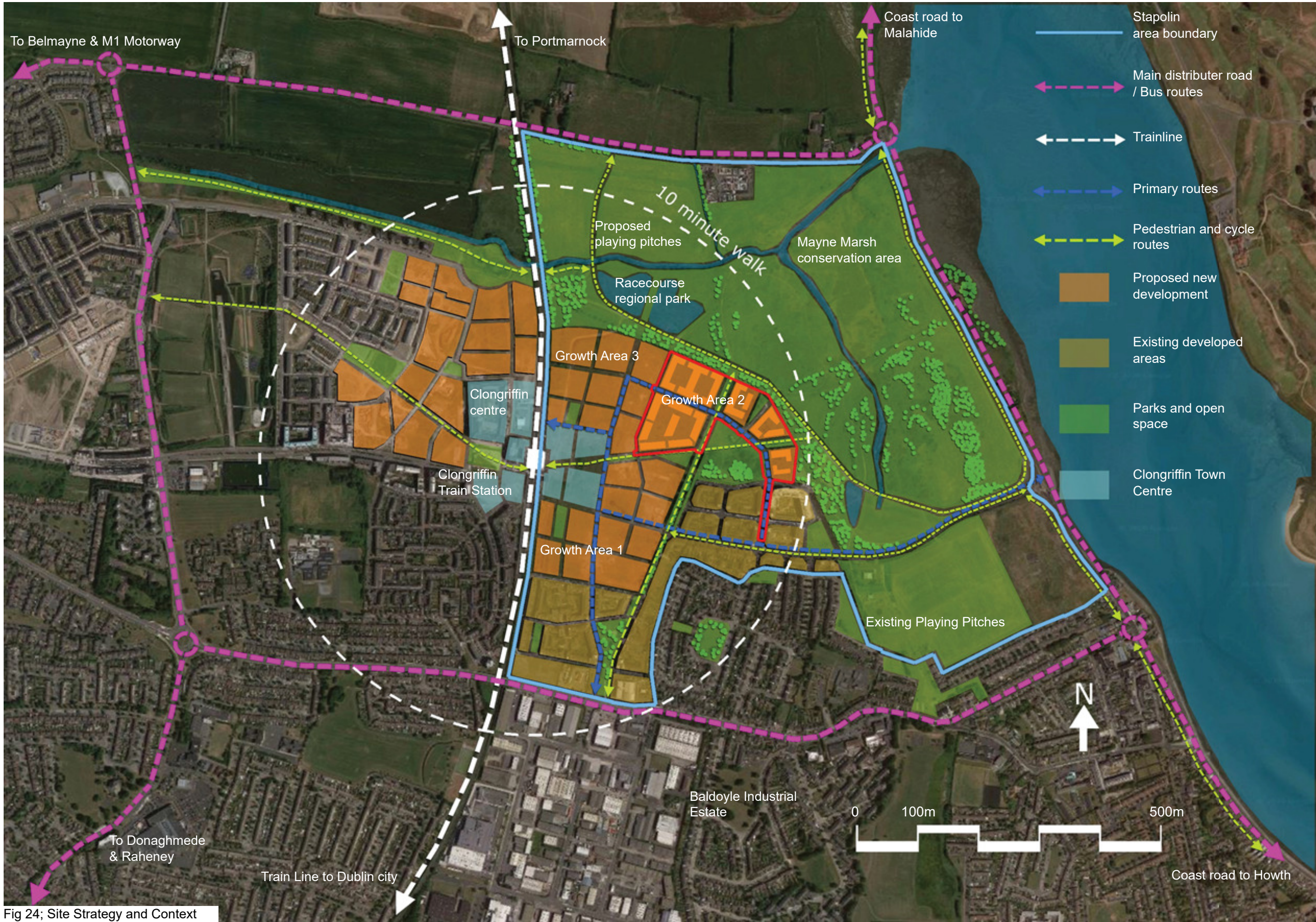


Fig 24; Site Strategy and Context

4.3 Inclusivity

4.3.1 Designing for Everyone

A mix of apartment types have been designed with a variety of integrated open spaces and courtyard parks to create places that can be enjoyed by all. The new home types range from studio apartments, to 1, 2 & 3-bed apartments. The scale and variety of apartments seeks to meet the aspirations of a range of people and households to create a balanced and sustainable community. Please refer to the Housing Quality Statement for more detail. The proposed mix would support a variety of people through all stages of their lives and accommodate residents who may desire to downsize within the neighbourhood.

4.3.2 Access for All

The overall site layout plan and the respective apartment building floor plans have been designed to afford a high level of accessibility for all future residents. Each apartment building is fully accessible at street level and from the basement car parking - all apartments and layouts comply with the Building Regulations Part M 2010. The Design Team have followed the principles of universal design to encourage access and use regardless of age, size, ability or mobility. All common lobbies have level access and have been designed in accordance with Part M. Accessible lifts are provided within each lobby. Stairs are designed as ambulant disabled straight stairs with no winders.

4.3.3 Open Space

The scheme provides for private, communal, and public open space in compliance with the development plan, LAP, and section 28 planning guidelines. A key feature of this scheme is the very generous communal courtyards at surface level that will promote interaction and foster a sense of community within the different building sectors. The courtyards benefit from passive surveillance from the proposed apartments and are secure being only accessible to the scheme residents and their visitors (figs. 25-29).



Fig 25 View between Sectors 8b & 8c showing “Irelands Eye Avenue” & proposed pedestrian link to Racecourse Park



Fig 26



Fig 27



Fig 28



Fig 29

4.4 Variety

4.4.1 Sector Types

The site comprises distinct development block as set out in the LAP. The shape, orientation, and position of these blocks within the overall site automatically provide for variety between the sectors, which affects the number of units and influences the proposed building heights in each sector (figs 30-32 illustrate). The following table provides an overview of the difference between the sectors in terms of building heights and the number of units:

Sector:	Building Heights:	Number of Units:
6a & 6b	5 - 6 storeys	335no. units
7	5 - 12 storeys	333no. units
8a	5 - 7, & 11 storeys	144no. units
8b	5 - 7 & 11 storeys	125no. units
8c	4, 5 & 8 storeys	70no. units
Totals:	4 - 12 storeys	1007no. units

Variety is thus achieved between the sectors despite commonality with the overall approach to the layout of each sector around a central courtyard.



Fig 30; Sector 8a



Fig 31; Sector 7



Fig 32; View south from Racecourse Park showing Building height variations

The variety within each sector is also achieved through the mix of apartments in the respective sectors. The following table provides a breakdown of apartment types per sector:

Sector:	Studio:	1-Bed Apartment:	2-Bed Apartment:	3-Bed Apartment:	Totals:
6a & 6b:	9	106	203	17	335
7:	28	60	236	9	333
8a:	10	44	83	7	144
8b:	11	24	83	7	125
8c:	-	13	52	5	70
Totals:	58	247	657	45	1007

With the exception of sector 8C, there is a full mix of studio, 1 bed, 2 bed and 3 bed units in each sector - there are no studio units proposed in sector 8C.

4.4.2 Apartment Types

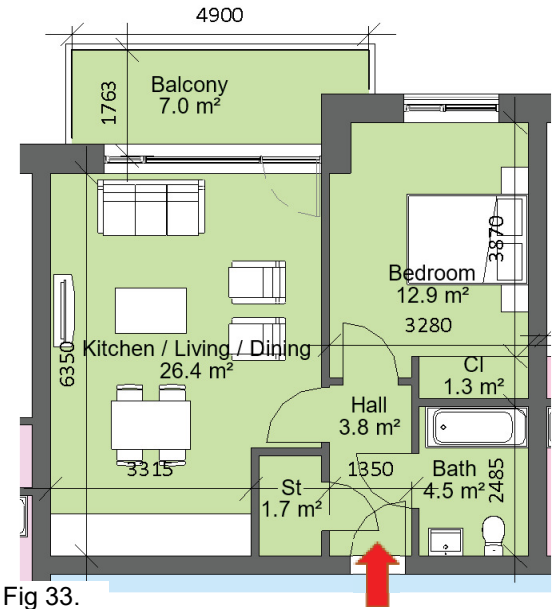
The proposal comprises 1,007 no. apartment units in 16 no. buildings. There are 5 different types of apartments proposed; studios, 1-bed units, 2-bed 3 person units, 2-bed 4 person units, and 3-bed units. Depending on floor area, orientation, floor level, etc, there is a very broad mix of apartment types and layouts within that mix. Some of these are illustrated opposite. The following table sets out the mix of units and floor area ranges per unit type:

Unit Type:	Mix of Units	Floor Area Range
Studio:	5.8% (58 units)	38.1 - 52.3 sq.m
1 Bed Units	24.5% (247 units)	48.9 - 79.7 sq.m
2 Bed 3 Person Units:	9.3% (94 units)	67.3 - 80.4 sq.m
2 Bed 4 Person Units:	55.9% (563 units)	77.7 - 106.1 sq.m
3 Bed Units:	4.5 % (45 units)	93.5 - 130.6 sq.m

4.5 Efficiency

4.5.1 Density

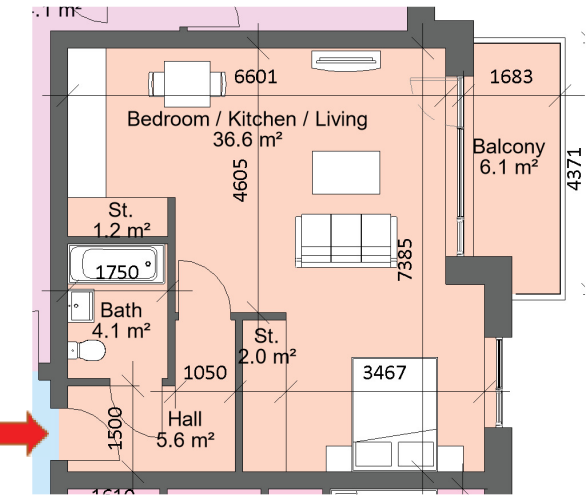
The scheme density seeks to create compact, strong, distinctive and successful communities at Stapolin. The development proposes 1,007 no. apartment units on a total site area of c. 6.1 hectares. The gross density of the residential development is approximately 165 units per hectare that is comparable with 177 uph for the approved GA3. Following the orientation and shape of the site, the scheme has been designed to cluster the highest density units to the northern edge of the site overlooking Racecourse Park. This arrangement provides for the courtyards to receive sunlight in compliance with the BRE Guidelines and it replicates the courtyard and height gradient approach adopted and currently approved on the site.



Typical 1-Bed Apartment

Ref; Sector 7, Block 1 Apartment 88

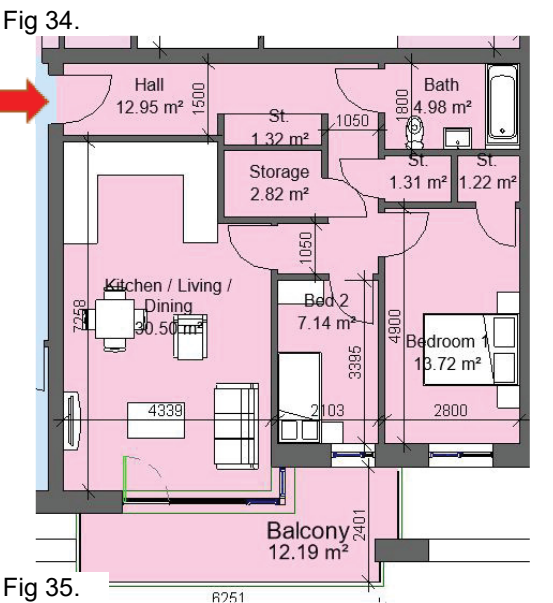
Area:	52.5sq.m
Aggregate Living:	26.4sq.m
Aggregate Bedroom:	12.9sq.m
Aggregate Storage:	3sq.m
Private Amenity:	7sq.m



Typical Studio Apartment

Ref; Sector 7, Block 1 Apartment 17

Area:	50.9sq.m
Aggregate Living:	36.6sq.m
Aggregate Bedroom:	-
Aggregate Storage:	3.2sq.m
Private Amenity:	6.1sq.m



Typical 2-Bed 3 Person Apartment

Ref; Sector 6a & 6b, Block 1 Apt 21

Area:	69.5sq.m
Aggregate Living:	28.4sq.m
Aggregate Bedroom:	21sq.m
Aggregate Storage:	5.1sq.m
Private Amenity:	9.0sq.m

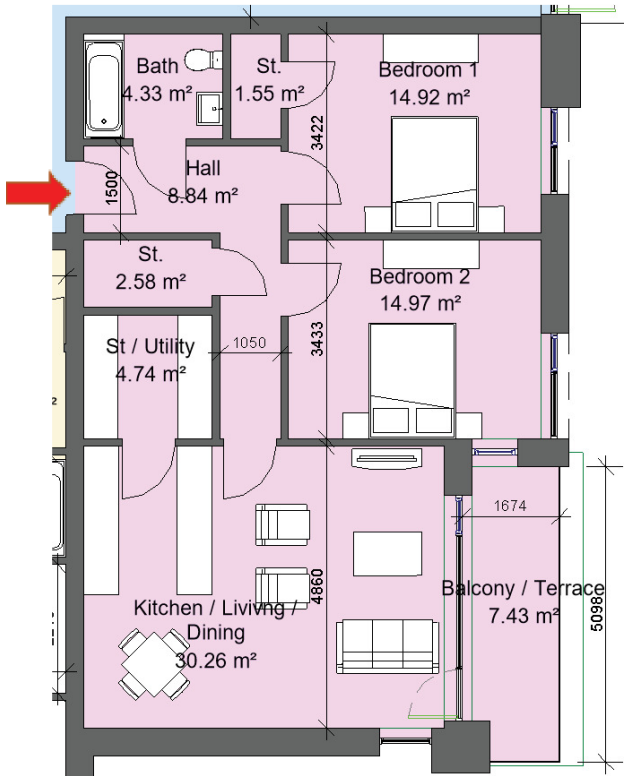


Fig 36.

Typical 2-Bed 4 Person Apartment

Ref; Sector 6a & 6b , Block 4 Apt 7

Area:	79.6sq.m
Aggregate Living:	30.2sq.m
Aggregate Bedroom:	23.5sq.m
Aggregate Storage:	7.1sq.m
Private Amenity:	7.4sq.m

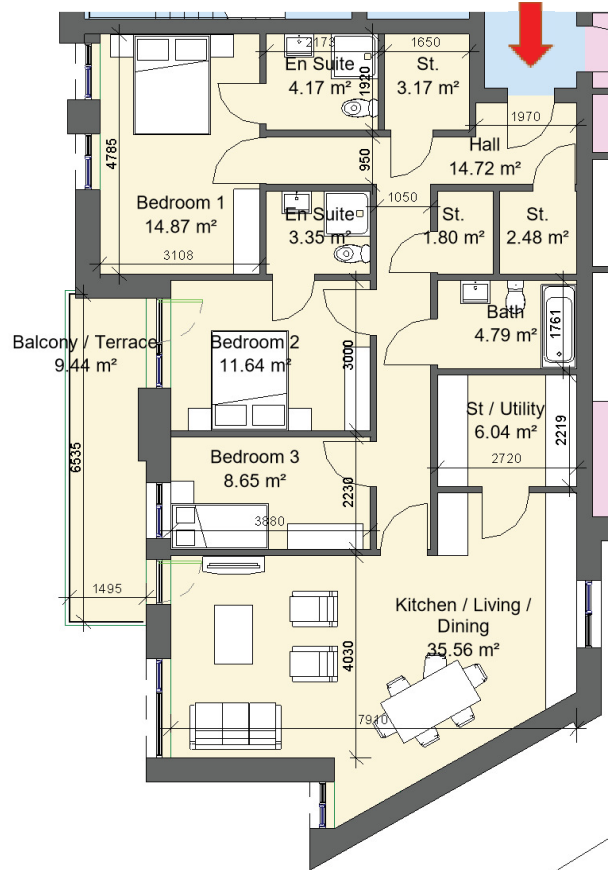


Fig 39.

Typical 3-Bed 5 Person Apartment

Ref; Sector 6a & 6b , Block 4 Apt 7

Area:	79.6sq.m
Aggregate Living:	35.5sq.m
Aggregate Bedroom:	35.2sq.m
Aggregate Storage:	13.5sq.m
Private Amenity:	9.4sq.m

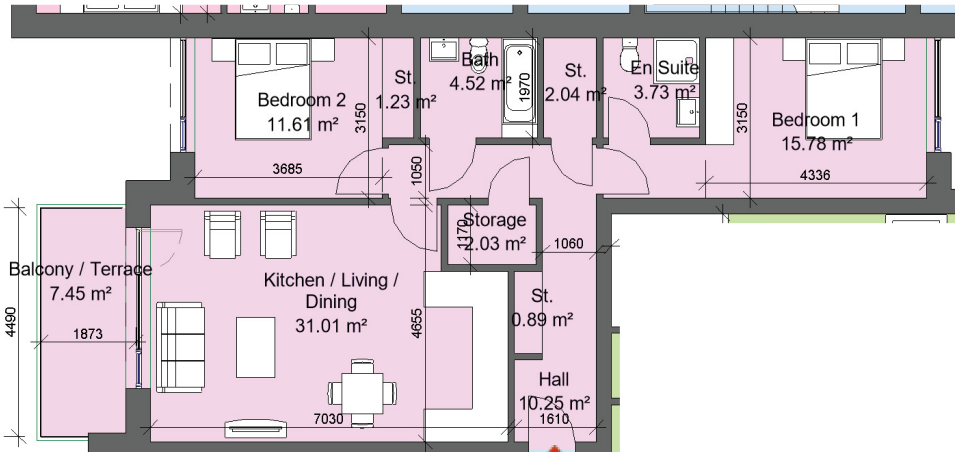


Fig 37.

Typical 2-Bed Dual Aspect Apartment

Ref; Sector 6a 6 6b, Block 1 Apt 5

Area:	87.2sq.m
Aggregate Living:	31.0sq.m
Aggregate Bedroom:	27.4sq.m
Aggregate Storage:	6.2sq.m
Private Amenity:	7.4sq.m

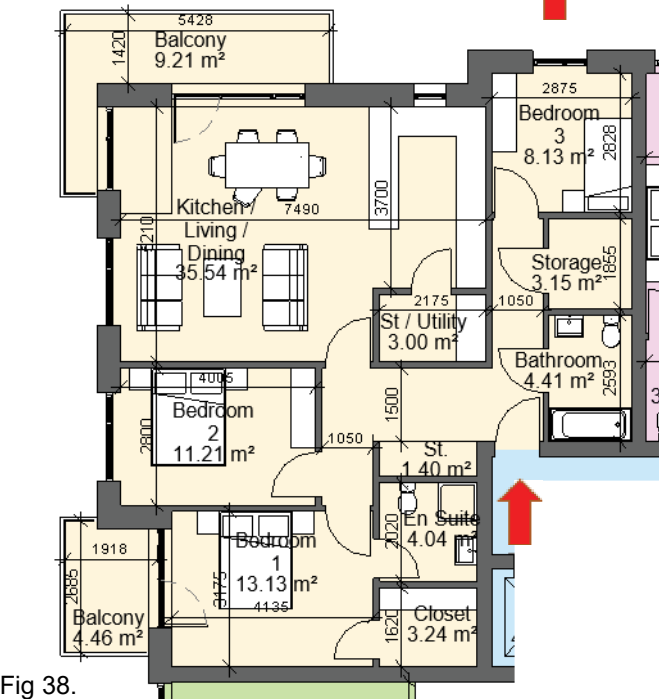


Fig 38.

Typical 3-Bed Dual Aspect Apartment

Ref; Sector 8c, Block 1 Apt 6

Area:	103.9sq.m
Aggregate Living:	35.5sq.m
Aggregate Bedroom:	32.5sq.m
Aggregate Storage:	9.4sq.m
Private Amenity:	13.7sq.m

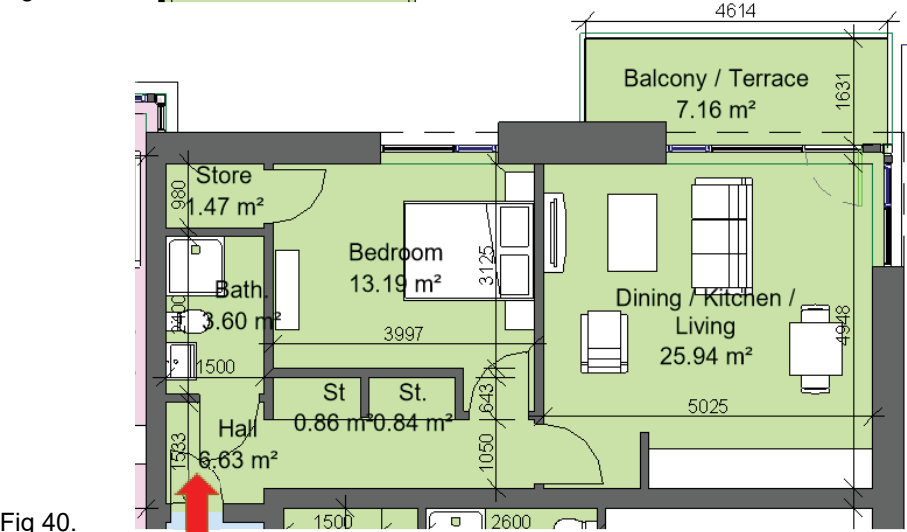


Fig 40.

Typical 1-Bed Apartment

Ref; Sector 6a & 6b, Block 1 Apt 3

Area:	53.7sq.m
Aggregate Living:	25.9sq.m
Aggregate Bedroom:	13.2sq.m
Aggregate Storage:	3.2sq.m
Private Amenity:	7.1sq.m

4.5.2 Building Heights

All development sectors benefit from their proximity and direct links to the adjacent parklands (proposed Racecourse Park) and Stapolin Haggard class 2 public open space. As a result, there is an opportunity for the design to utilise the immediate site context to pursue taller buildings that address and overlook those expansive open spaces. The scheme provides for a range of building heights between 4 and 12 storeys with the lower heights positioned along the southern boundary of the various building sectors and the taller building towards the north (fig. 41).

The proposed building heights are cognisant of the extant 5 storey apartment development to the south at Red Arches and the approved GA1 and GA3 schemes that include buildings up to 15 storeys high. The mix of building heights is intended to provide visual interest to the parkland edge, encourage variety and distinction within the development sectors, and marry with the overall emerging urban development at the Coast. (figs 42 & 43)

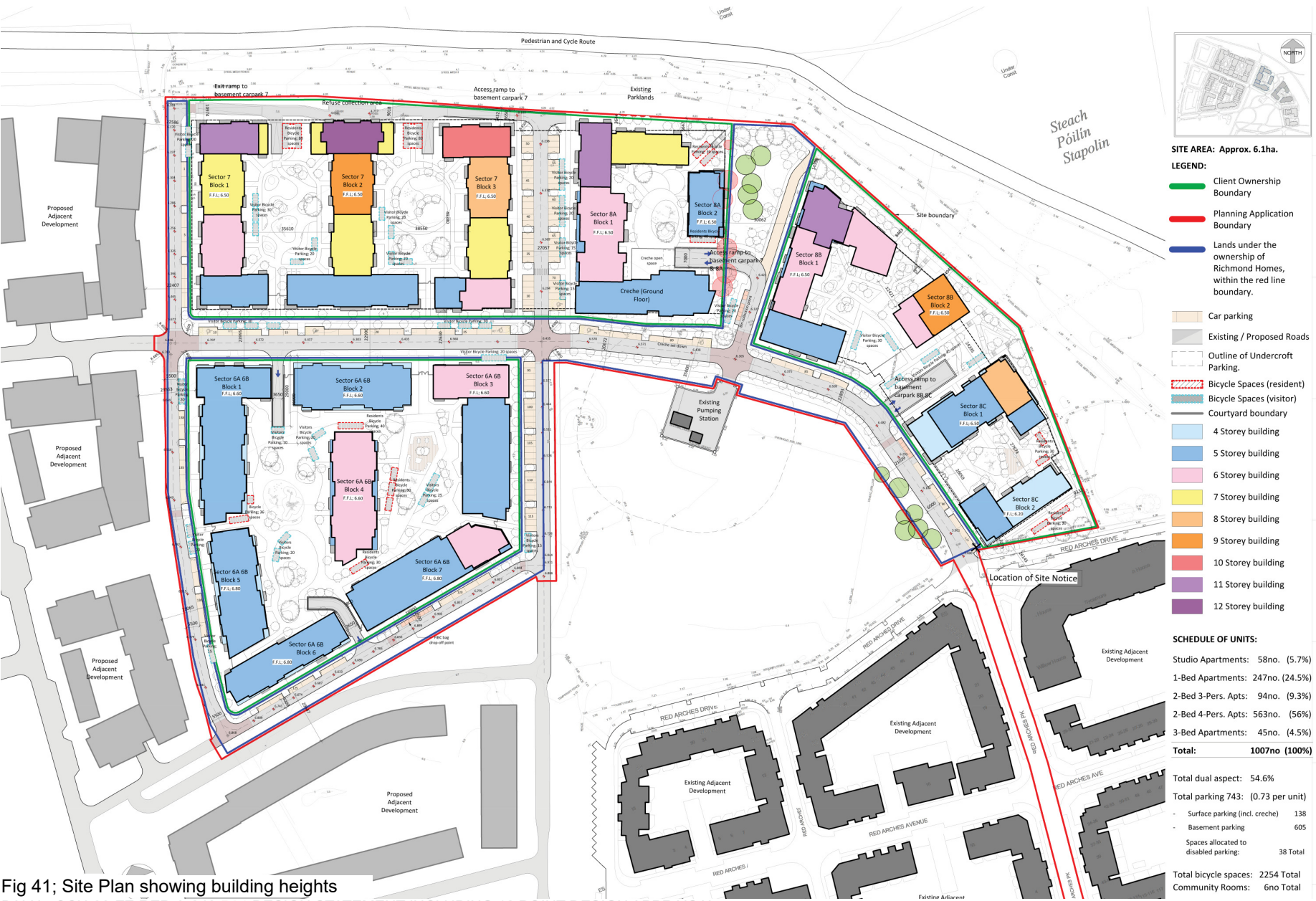


Fig 41; Site Plan showing building heights
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Fig 42; Site Density diagram



Fig 43; Site strategy diagram

4.5.3 Recycling Facilities

There is provision for a three-bin system to be used throughout the scheme. The apartments are provided with secure compactor rooms and Flexible Intermediate Bulk Container (F.I.B.C.) bag storage rooms in the basement areas. Bin stores are to be managed by the Management Company and easily accessible to all apartment residences. Locations and designs of bin stores are to facilitate easy maintenance, without causing a nuisance to adjacent apartments or residences. Bin stores for the crèche are located adjacent to the public road for ease of service and access. Further information on waste management can be found in AWN Consultants Waste Management Report. The maintenance and collection will be within the remit of the management company.



Fig 44; Basement recycling facilities

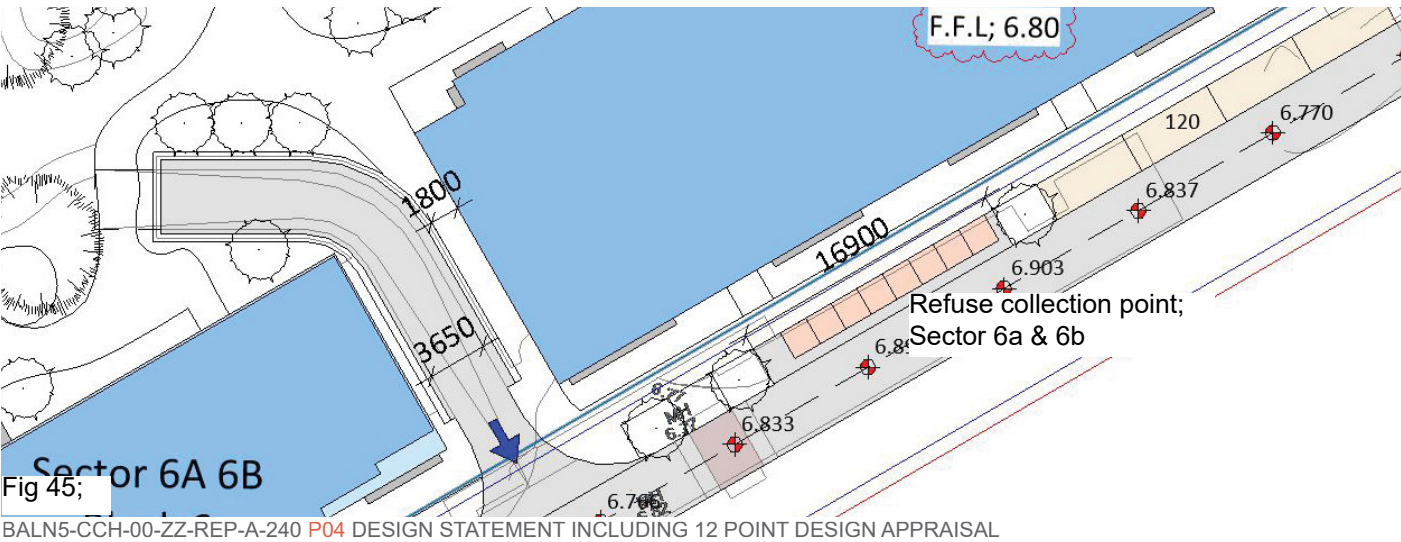


Fig 45;

4.5.4 Sustainable Drainage

The SuDS strategy adopted for the proposed development provides a comprehensive approach to the management of storm water on the site in line with the SuDS triangle namely, water quality, water quantity and amenity/biodiversity. The treatment train approach has been adopted for the design of the storm water system for the development. This approach uses suitable SuDS measures in providing source, site and regional controls. The SuDS recommendations included in the Baldoyle Stapolin LAP have been assessed and have been included where deemed appropriate and suitable for this development. The storm water wetland is included as one of the essential SuDS measures for the development.

SuDS measures proposed, within the curtilage of dwellings, include the following:

- Green Roofs
- Podiums including Permeable Paving

SuDS measures proposed as site controls within public road carriageways and the public open space include the following:

- Bio-retention areas/tree pits
- Swales running parallel to road carriageways/footpaths
- Filter drains running parallel to footpaths
- Silt and Hydrocarbon interceptors for road carriageways/carpark areas



Fig 46;



Fig 47;

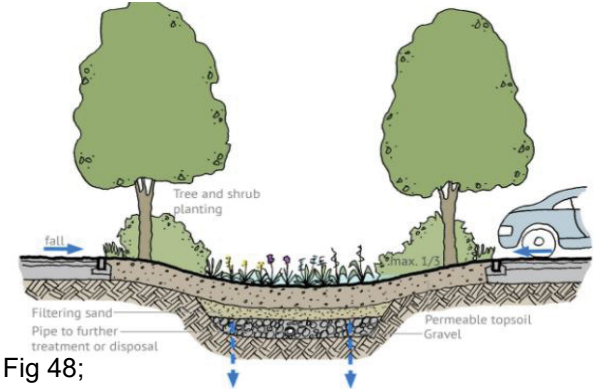


Fig 48;



Fig 49;

4.6 Distinctiveness

4.6.1 Character Areas

The site includes four character areas as set out in the LAP; part of The Northern Residential Area; Sector 6A/6B (1), part of The Racecourse Park Area; Sector 7 (2), and The Haggard Area; Sectors 8A, 8B, & 8C (3). In addition, the communal courtyards constitute a fourth character area (4) within the scheme. Each of the sectors is examined in detail on the following pages.

The LAP character areas will create a sequence of spaces and routes, about which residential neighbourhoods will evolve, linking with existing residential and development areas, and with existing public transport links.

Landscaped courtyards are a key feature of the scheme and are located securely within apartment blocks. These spaces are easily accessible to apartments as well as to pedestrians and cyclists (residents) coming in from the street. As part of the development of courtyard gardens, Murray & Associates have proposed an attractive landscape design.



Fig 50; Sector 7; courtyard illustration



Fig 51; Sector 6a & 6b; courtyard illustration



Fig 52; Sector 6a & 6b; Plan diagram

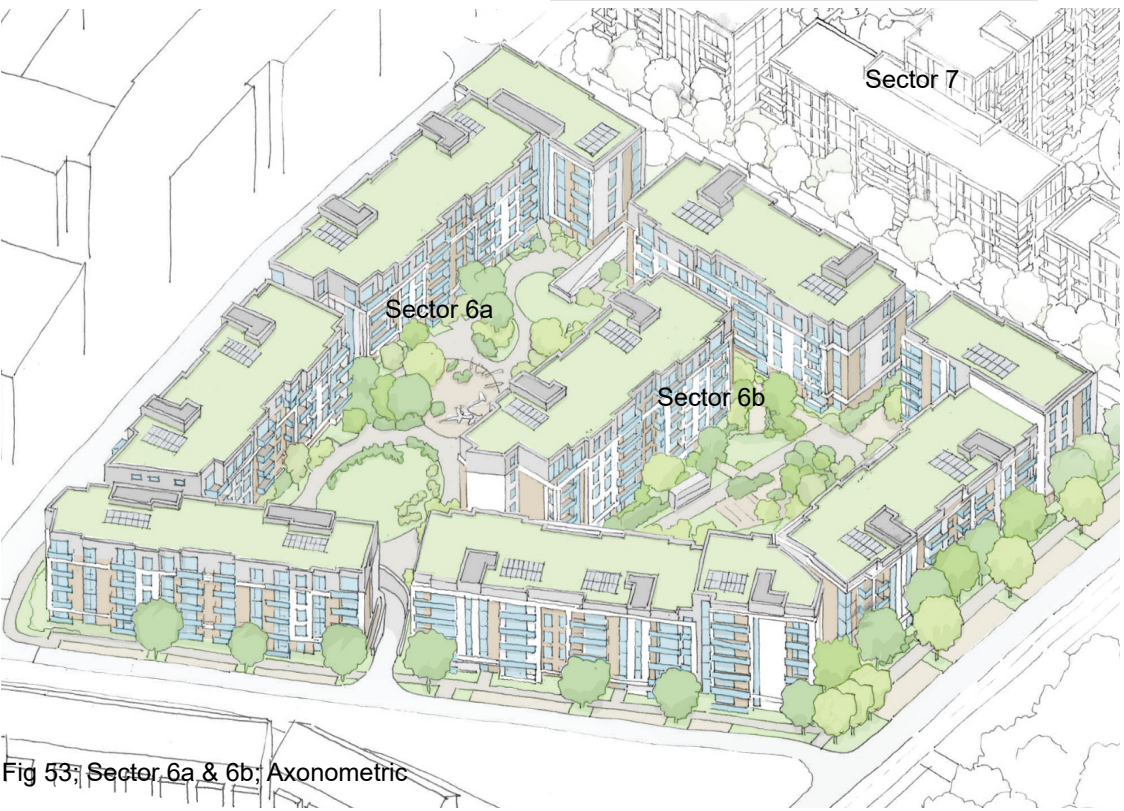


Fig 53; Sector 6a & 6b; Axonometric



Fig 54; Sector 6a & 6b; Sector plan

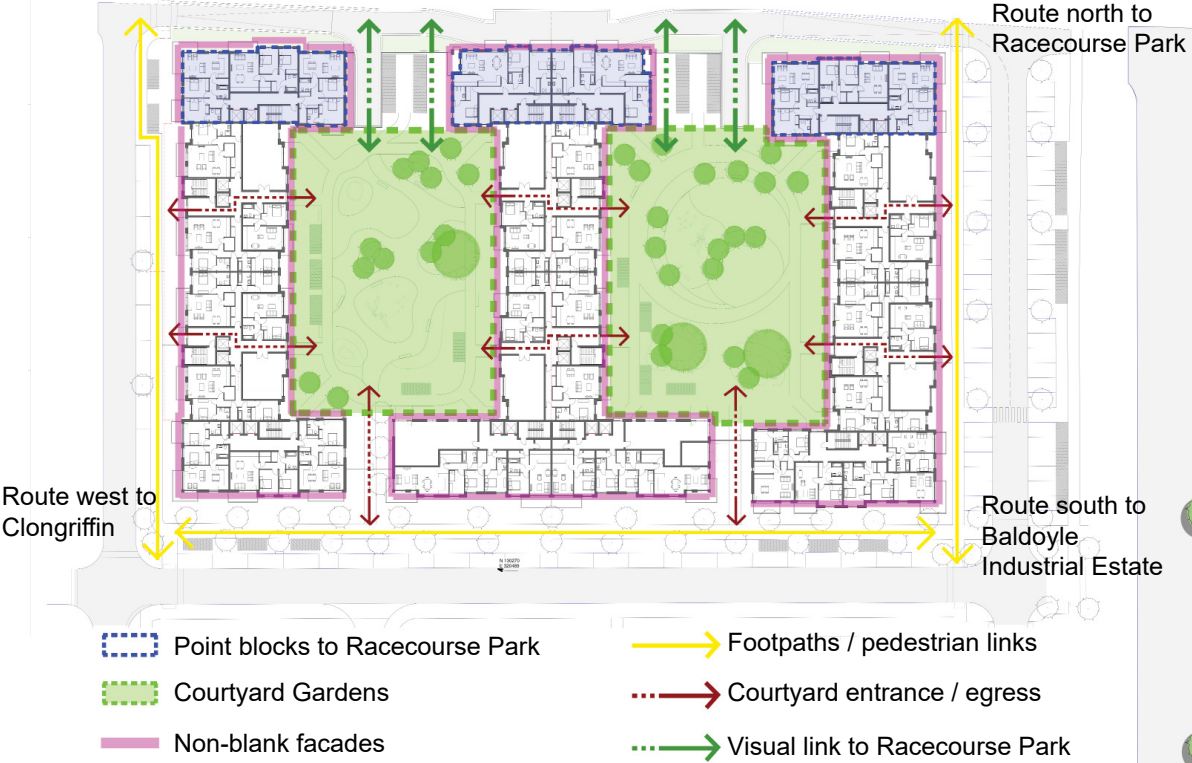


Fig 55; Sector 7; Plan diagram



Fig 57; Sector 7; Axonometric



Fig 56; Sector 7; Sector plan

- 1 Bed Apartment
- 2 Bed Apartment
- 4 person
- 3 Bed Apartment
- Bicycle store rooms
- Sub-station
- Dual-aspect units
- Apartment bldg. entrance
- Pedestrian courtyard access

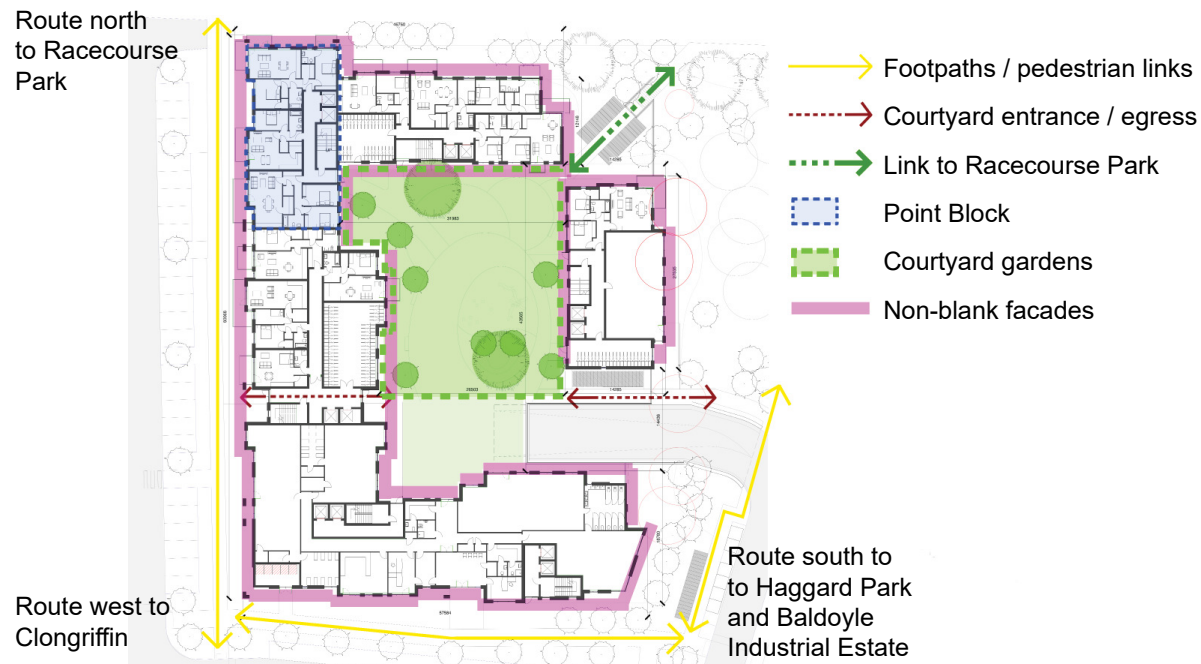


Fig 58; Sector 8a; Plan diagram



Fig 60; Sector 8a; Axonometric

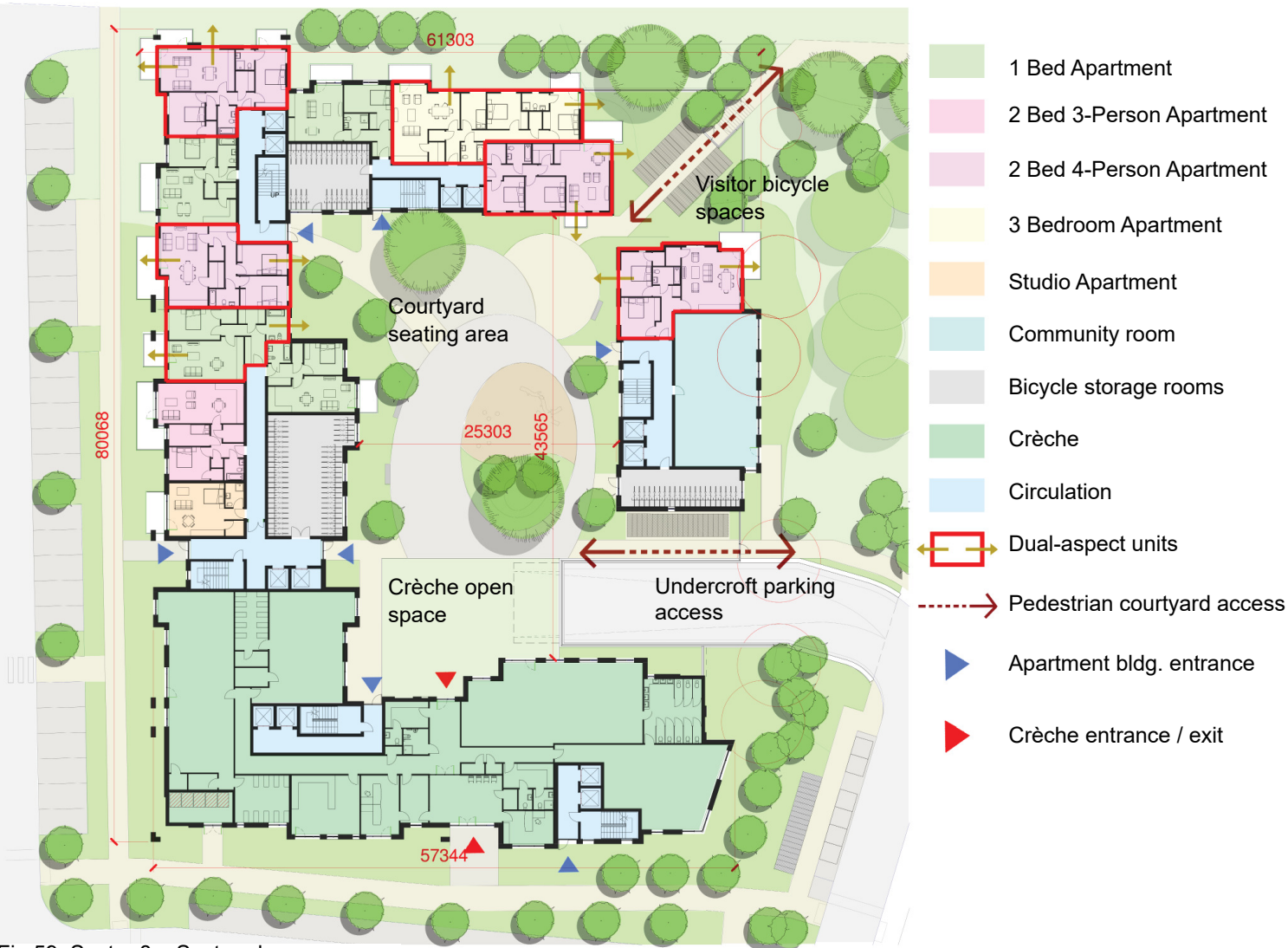


Fig 59; Sector 8a; Sector plan



Fig 61; Sector 8b; Plan diagram

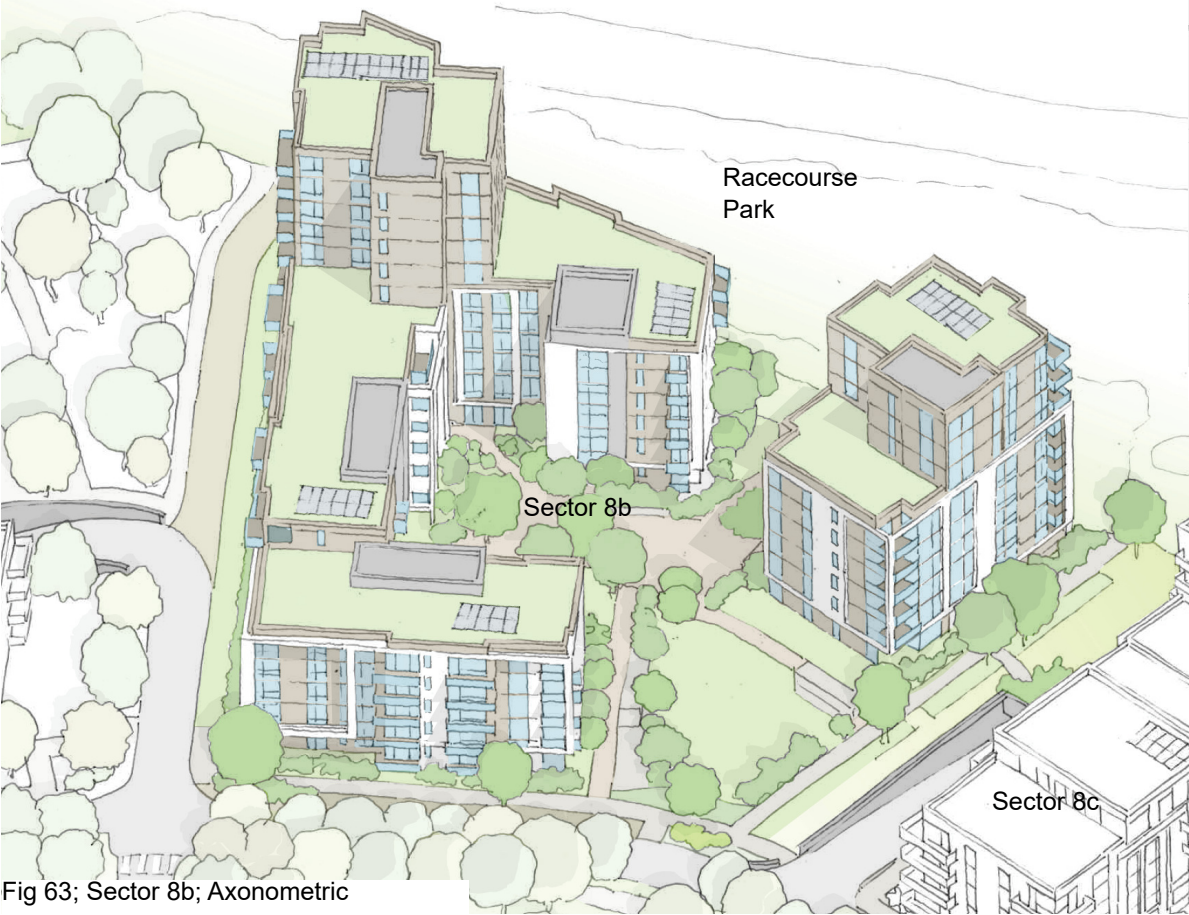


Fig 63; Sector 8b; Axonometric



Fig 62; Sector 8b; Sector Plan



Fig 63; Sector 8c; Plan diagram

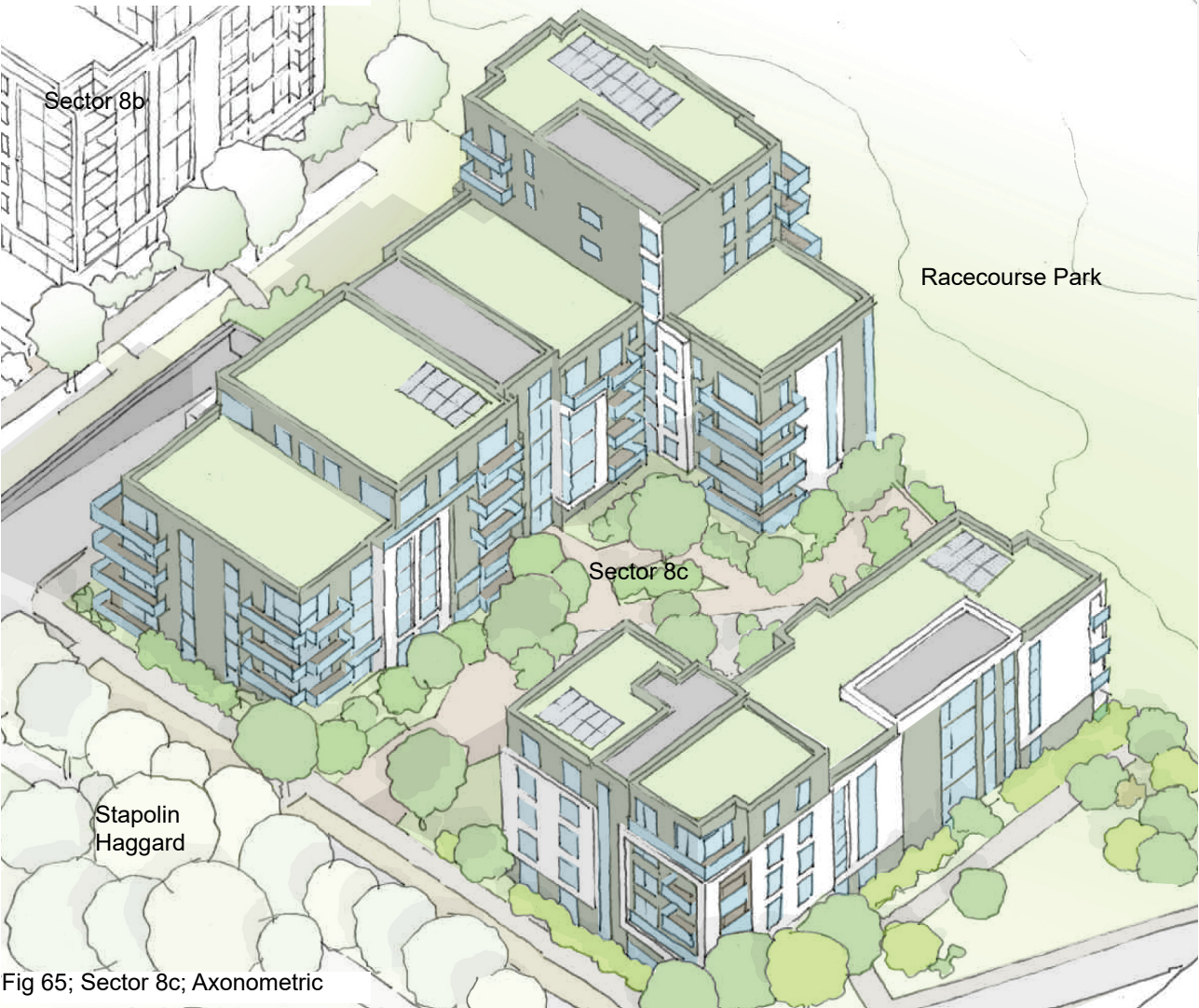
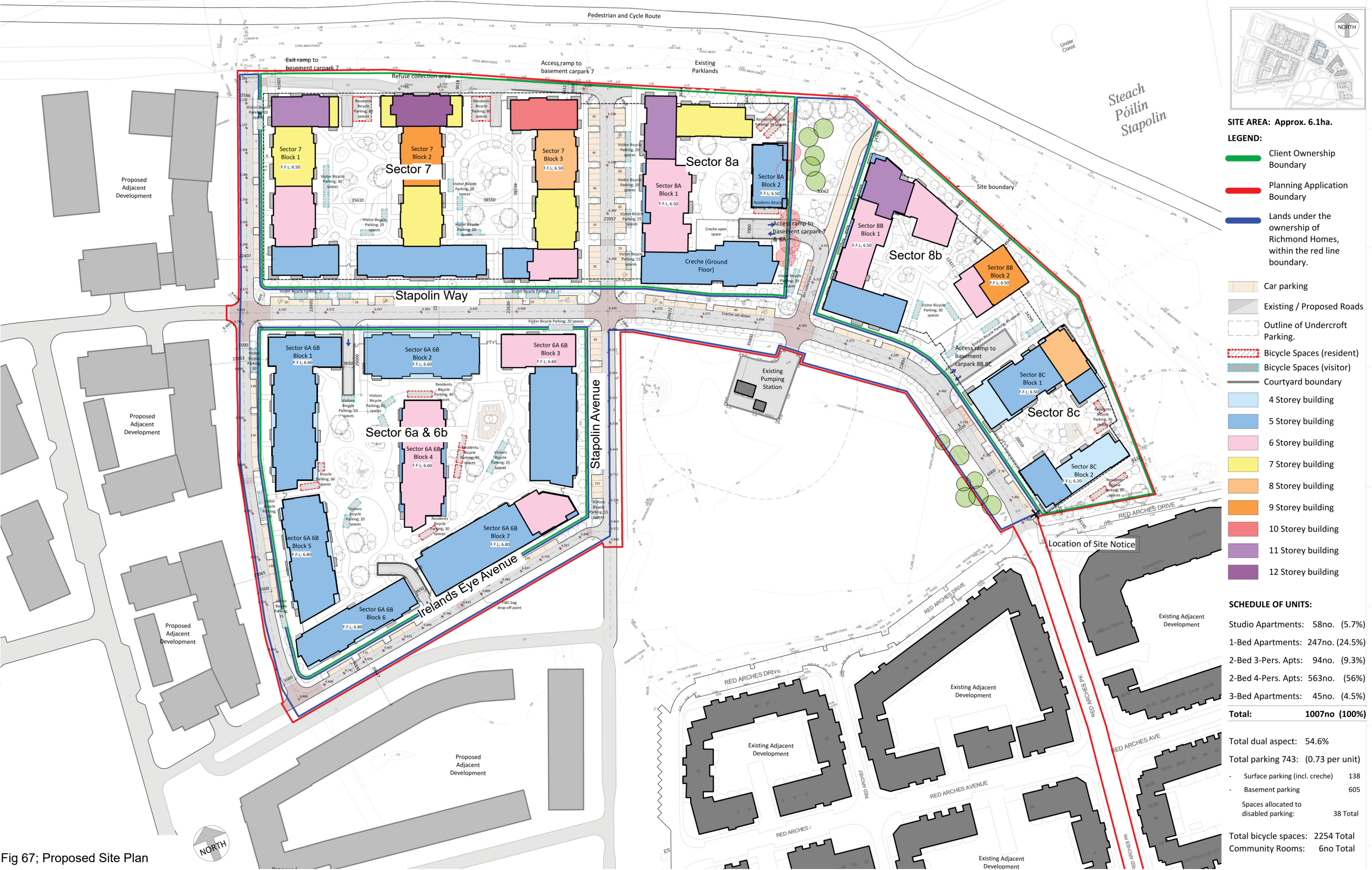


Fig 65; Sector 8c; Axonometric

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Fig 66; Sector 8c; Sector plan



SITE AREA: Approx. 6.1ha.

LEGEND:

- Client Ownership Boundary
- Planning Application Boundary
- Lands under the ownership of Richmond Homes, within the red line boundary.
- Car parking
- Existing / Proposed Roads
- Outline of Undercroft Parking.
- Bicycle Spaces (resident)
- Bicycle Spaces (visitor)
- Courtyard boundary
- 4 Storey building
- 5 Storey building
- 6 Storey building
- 7 Storey building
- 8 Storey building
- 9 Storey building
- 10 Storey building
- 11 Storey building
- 12 Storey building

SCHEDULE OF UNITS:

Studio Apartments:	58no. (5.7%)
1-Bed Apartments:	247no. (24.5%)
2-Bed 3-Pers. Apts:	94no. (9.3%)
2-Bed 4-Pers. Apts:	563no. (56%)
3-Bed Apartments:	45no. (4.5%)
Total:	1007no (100%)

Total dual aspect: 54.6%

Total parking 743: (0.73 per unit)

- Surface parking (incl. creche) 138
- Basement parking 605

Spaces allocated to disabled parking: 38 Total

Total bicycle spaces: 2254 Total

Community Rooms: 6no Total

4.7 Layout

4.7.1 Proposed Site Layout

The proposed development layout (fig. 67) has been carefully arranged within the established building block layout to create a permeable interconnected series of routes aligned with desire lines and view corridors that extend and integrate with adjacent established and proposed residential communities at Stapolin Baldoyle. The overall masterplan of the surrounding area requires our proposal to be integrated with any potential future developments to the south and west in a manner that is easy and logical to navigate around. To a large extent, this is achieved by compliance with the overall road hierarchy that is set out in the LAP and the building blocks within the application site.

A variation to the LAP indicative layout is proposed to sectors 6A/6B where an enlarged sector is planned and a minor local road is omitted. This replicates the approved approach to sector 7 and enables increased separation between opposing apartments and more communal open space within the courtyard.

The overall strategy was developed around multifunctional spaces utilising shared surfaces, varied parking arrangements and graduated building lines. This approach provides a series of contemporary apartment arrangements around communal courtyard with a distinct urban feel while still affording the residents a sense of community and identity within their sectors. Within the development blocks, lower building heights are planned at the southern end of the courtyard with the taller buildings to the north. This is to ensure sufficient sunlight within the courtyards.

4.7.2 Road Hierarchy

The existing development on the site and as repeated in the LAP, provides for a clear road hierarchy within the overall Coast development. This hierarchy includes two green routes, a secondary (road) route, and a quiet street through this site.

The two green routes are the two boulevard avenues within the LAP area. The first, Ireland's Eye Avenue, links the proposed town centre with Stapolin Haggard and the view of Ireland's Eye to the northeast. The second, Stapolin Avenue, runs approximately north-south from the access at Grange Road through to Racecourse Park between sectors 7 and 8A. The approved development at GA1 to the south has been approved with an amended road design for Stapolin Avenue, so this has been replicated within our layout. Ireland's Eye Avenue is set out in compliance with the LAP.

The secondary route, Stapolin Way, is arranged around the periphery of Stapolin Haggard. It divides the site with sectors 7, 8A, 8B and 8C to the north, and sector 6A/6B to the south, and links Red Arches Road to the south with Longfield Road. This road is already in place being constructed on foot of planning permission F03A/1529.

4.7.3 Pedestrian and Cyclist Connectivity

This proposal combines a contemporary mix of urban streetscapes to promote pedestrian and cyclist connectivity within the public and semi-private realm.



Fig 67; Bicycle parking provision

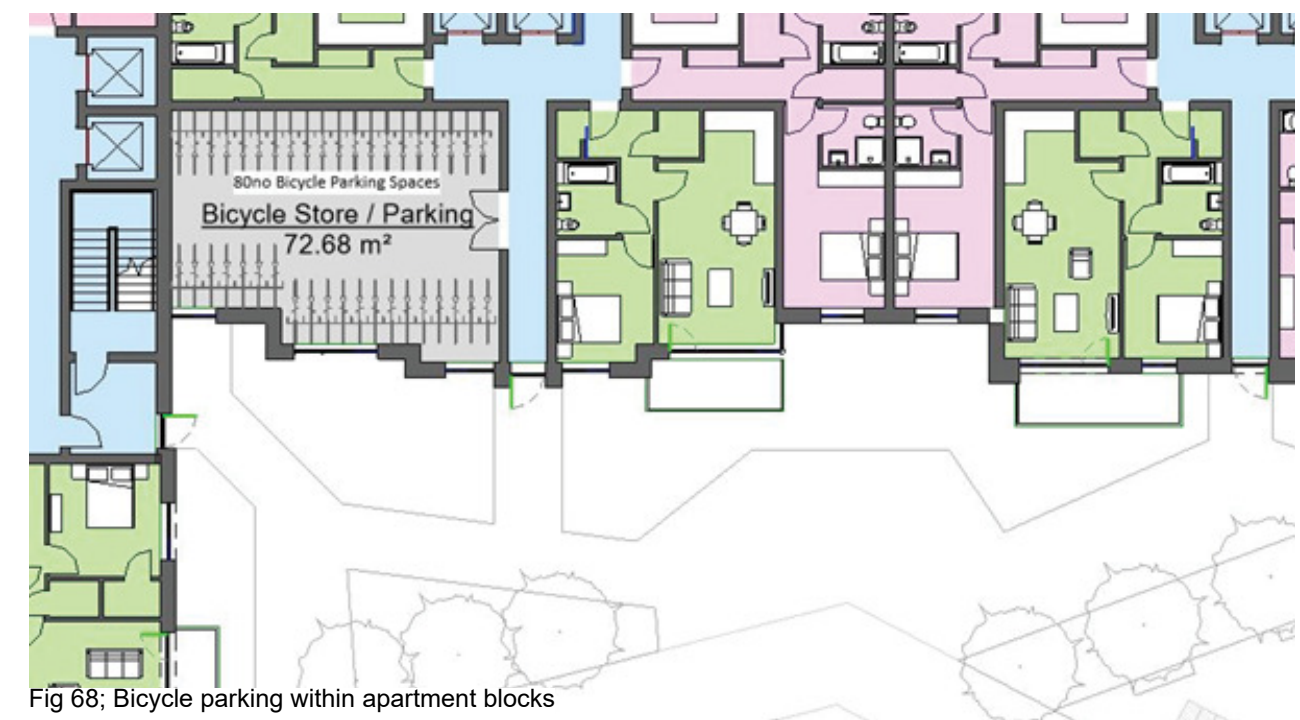


Fig 68; Bicycle parking within apartment blocks

As well as routes encompassing the proposed development, visual and pedestrian friendly links are designed to traverse the site, with tree-lined routes and pedestrian pathways connecting the various sectors to the smaller and larger open spaces. Footpaths are provided along the edges of roads beside all apartment blocks, within the courtyards, and open green areas.

Proposed routes to Racecourse Park and beyond will serve the scheme as sustainable transport links by means of pedestrian and cycle routes woven through a distinctive and bio-diverse landscape (fig. 24). The greenways running north-south and east-west will allow connections between these residential areas and commercial areas, including the approved local centre adjacent to Clongriffin DART Station to the west and Baldoyle Industrial Estate to the south.

The priority also given to cycling is demonstrated by the scheme provision of 1 no. bicycle parking space per proposed bedroom plus 1 no. visitor bicycle space per 2 no. units i.e. a total of 2,254 no. bicycle parking spaces. These spaces are contained within secure storage areas at ground floor locations throughout the scheme and within the communal courtyards which are secure, gated enclosures. Visitor bicycle parking spaces are located off-street in areas of passive surveillance (figs 67 & 68).

4.8 Public Realm

4.8.1 Public Open Spaces

The proposed layout has been carefully arranged to create communal courtyard for the scheme residents and to provide passive surveillance to Stapolin Haggard and proposed Racecourse Park.

The courtyards are generously sized and will serve as communal open space, so that each apartment has ready access to high quality amenity space within each sector. The design and layout of these landscaped spaces have been prepared by Murray & Associates – see their report for more detail (example, fig. 69).

4.9 Adaptability

While the expansion of apartments are restricted to the confines of their envelope, the interiors of each apartment has been designed with a flexible approach in mind, to allow for future adaptability. Opportunities exist to amalgamate adjoining units. Practice elsewhere suggests this is rarely pursued but the apartment floor plan layouts do provide for adjoining studio and 1-bed units to enhance their adaptation to a larger single unit.

Each apartment will achieve the minimum energy performance in accordance with current building regulations, including NZEB standards for all dwellings occupied after the 31st December 2020.

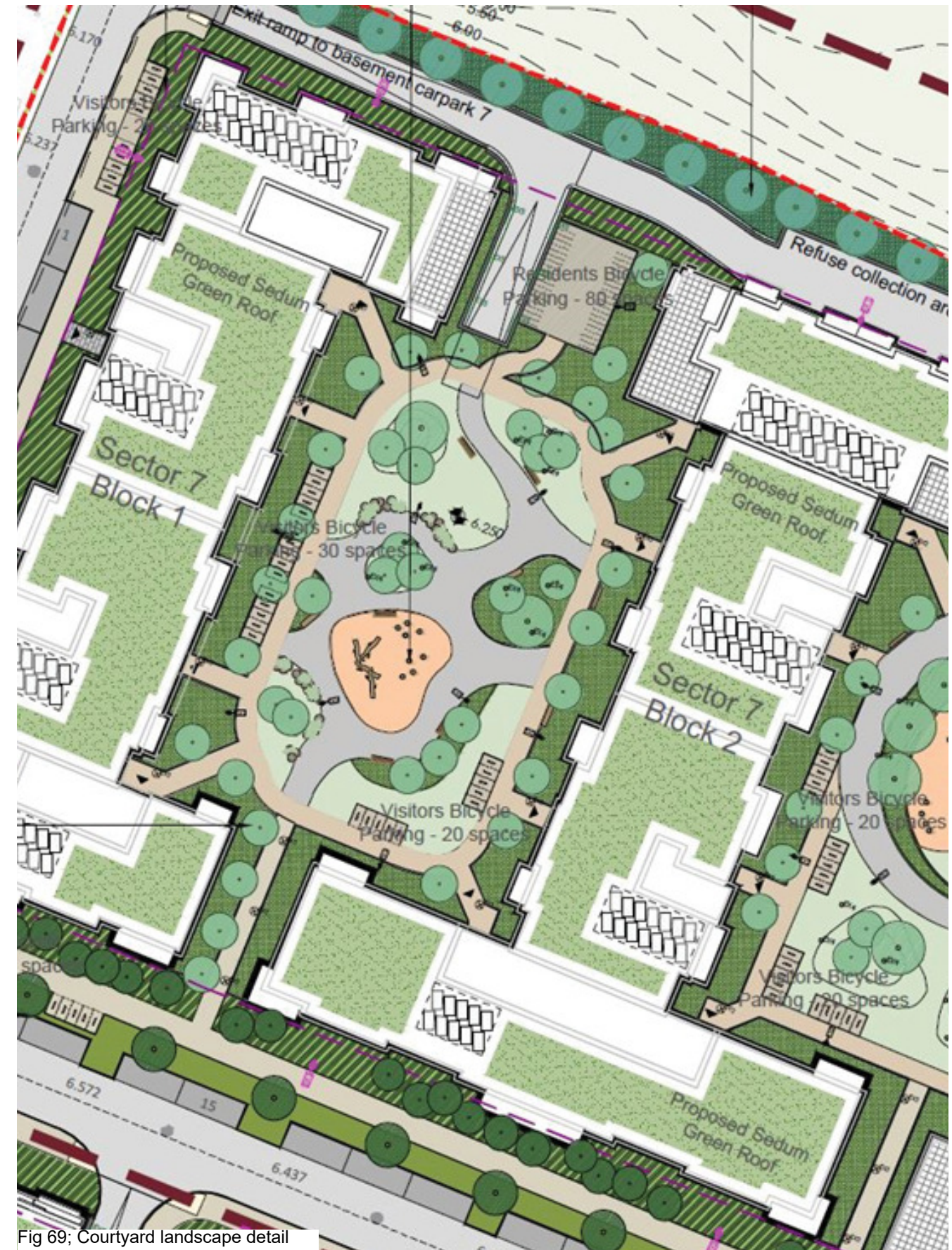


Fig 69; Courtyard landscape detail

4.9.1 Universal Access;

Case Study; Sector 8c, Block 1, Apartment 1.

- 1- Level internal corridor and level entry to each apartment. Communal corridors 1800mm, or min. 1500mm with 1800mm wheelchair turning circle on all levels.
- 2- Entrance hall. Minimum 1500mm (or min. 1300mm) with adjacent closet storage.
- 3- All doors to accessible rooms have a minimum clear width of 800mm. 300mm clear space on leading edge. Level transition at all doors.
- 4- Bathroom designed in accordance with Part M of T.G.D.(2010) incorporating wheelchair space of 1200x750mm.
- 5- Bedrooms designed in accordance with the “Sustainable Urban Housing; Design Standards for New Apartments (2020). Minimum bedroom width of 2900mm. Master Bedroom is min. 13sq.m. Secondary double bedroom is min. 11.4sq.m.
- 6- Kitchen layout sufficient to accommodate a 1500mm turning circle between work counters. Living area is dual aspect in this case. Where single aspect, care has been taken to keep the room depth to a minimum of 7.5m in accordance with BRE guidelines regarding to daylight.
- 7- Each apartment is provided with a balcony or terrace with level access. This is set back from public footpaths by a minimum 1500mm in accordance with apartment guidelines, and screened at ground level with planting. Balcony width is a min. 1500mm in all cases. Open space compliance in accordance with apartment standards.
- 8- Minimum corridor width of 1050mm in accordance with TGD Part M.
- 9- Generous floor to ceiling height of approx. 2.9m. Generous floor to ceiling glazing of 2.5m.
- 10- Privacy strip between the ground floor apartments and public footpaths of min. 1.5m. Privacy strip to be landscaped as per details by Murray & Associates landscape architects.
- 11- Width of Living Room is min. 3.3m in all cases, in accordance with Design Standards for New Apartments (2020).

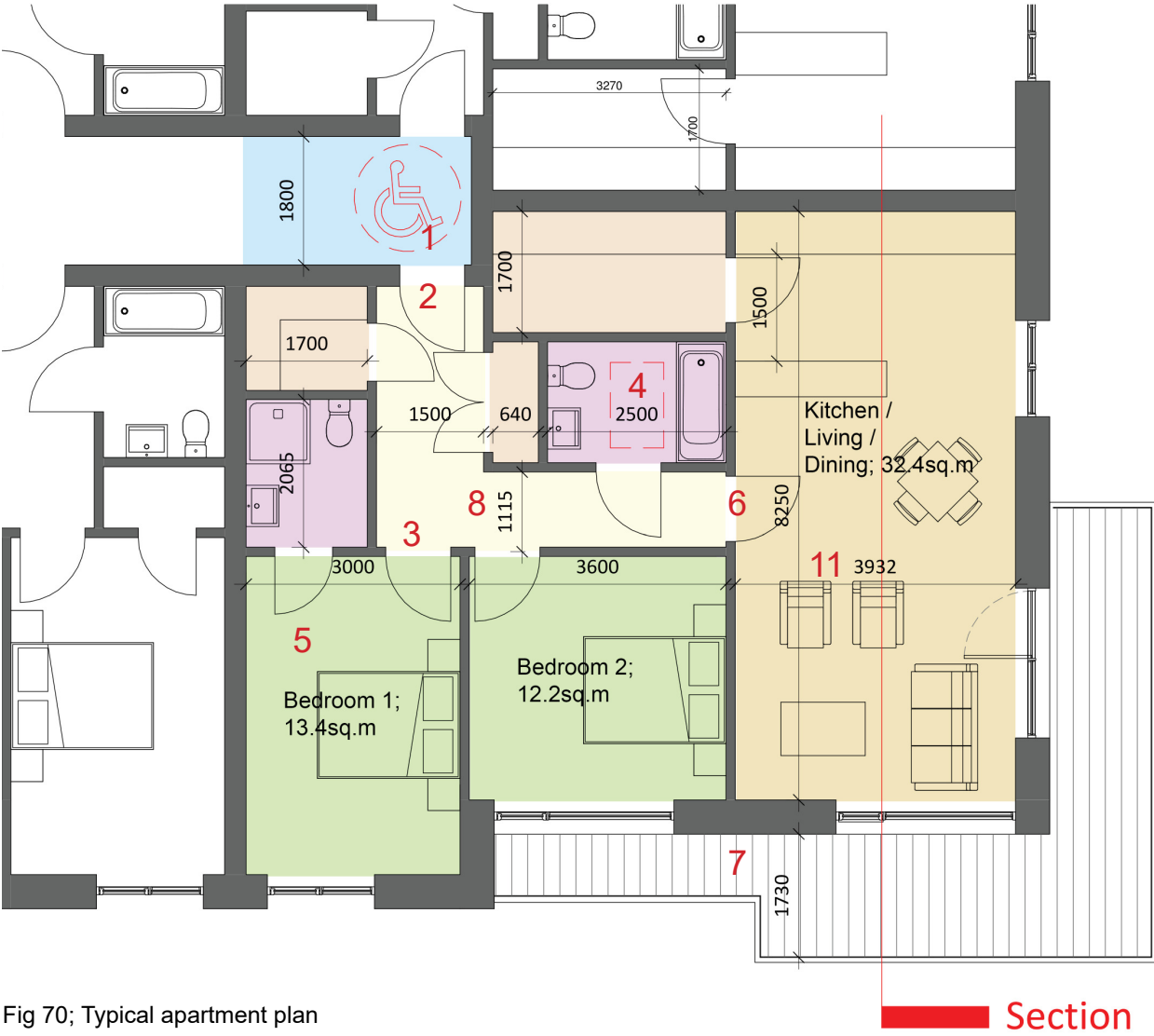


Fig 70; Typical apartment plan

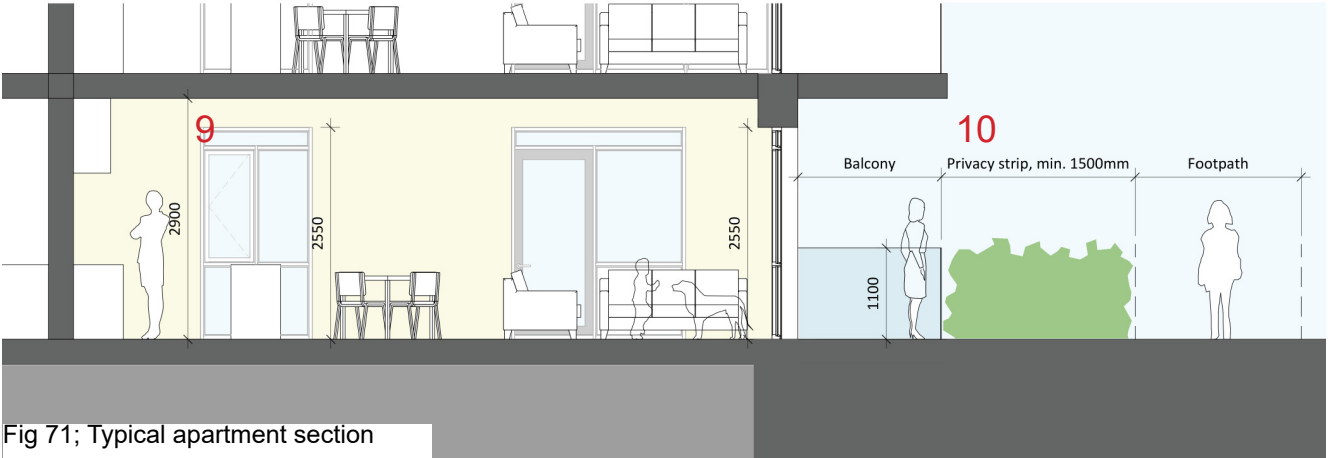


Fig 71; Typical apartment section

4.9.2 Dual Aspect Units

Dual aspect units achieve a total of 54% of the total units, exceeding the development plan guidelines of 30% dual aspect units. The breakdown of dual aspect units per sector is as follows:

- Sector 6a & 6b; 162no units (48% of total units in this sector)
- Sector 7 190no units (57% of total units in this sector)
- Sector 8a 73no units (51% of total units in this sector)
- Sector 8b 77no units (65% of total units in this sector)
- Sector 8c 48no units (69% of total units in this sector)

- **Total: 550no units (54% of total units throughout the development)**

4.10 Privacy and Amenity

4.10.1 Privacy

At ground floor level, privacy of the units is achieved by a substantial setback distance separating the apartments from the public road or footpath with soft landscaped barriers between ground floor balconies and footpaths. The separation provides for good visibility of the public realm for occupants, whilst maintaining a suitable level of passive surveillance of public areas. At the upper levels, privacy within the development is achieved through careful positioning of windows and balconies and by maximising the window to window separation between opposing apartment units. This results in large communal courtyards within the development blocks. One of the best examples of this approach is sector 6A/6B which has been amalgamated to increase the separation distances across the courtyards. These are c. 35 metres across the narrow part of the courtyard, which comfortably exceeds the 22 metres standard (fig. 72).

4.10.2 Private Amenity

Each apartment unit has dedicated private amenity space in the form of a terrace at ground floor level or balcony at the upper levels. For every apartment, the private open space provision complies with the standards set out in the Apartment Guidelines (December 2020) and are fully detailed in the Housing Quality Statement. These private amenity spaces maintain privacy between neighbouring apartments within the same block but are not overlooked by neighbouring apartments. The glazed balustrades to the balconies and patio areas provide ample privacy without creating hard boundaries. It was also a key design requirement that as many apartment units are dual aspect design, which enhances the daylight provision within the units that in turn enhances the amenity of the apartment. The apartment scheme has c. 54% dual aspect units which is fully in compliance with the Department of the Environment Design Standards for New Apartments 2020. The proposed buildings, communal public spaces and private balconies are arranged, where possible, to maximise solar orientation. The fenestration of individual apartments has been designed to take account of maximum solar gain, while communal courtyard gardens are laid out to achieve an optimum amount of direct sunshine. As confirmed by BRE, the sunlight provision within the courtyards all exceed the 2 hours for at least 50% on the 21st March.



Fig 72; Sector 6a; courtyard view



Fig 73; View looking south-east along Stapolin Way



Fig 74; View of Sector 8c from Racecourse Park

4.11 Parking and Supervision of Parking

4.11.1 Car Parking Provision

A mix of car parking arrangements is provided within the scheme to reflect the varied nature of the proposed housing. The scheme will provide 743 no. car parking spaces which equates to 0.7 spaces per dwelling unit. Of these parking spaces, 138 no. spaces are at ground level i.e. on-street with the remaining 605 no spaces located throughout the basements. 14 no crèche set-down spaces have been allocated in direct response to the pre-application consultations comments from Fingal County Council and An Bord Pleanála.

In accordance with Technical Guidance Document Part M (Access and Use 2010) of the Building Regulations, there is c. 1 in 20 parking spaces are reserved for mobility impaired motorists. These are located throughout the scheme adjacent to footpaths in areas which are easily accessible from the apartment buildings. Any mobility impaired spaces at the basements are located adjacent to lift cores for ease of access.

The on-street car parking areas are located within the vicinity of individual apartment blocks and would be overlooked by residents. Visitor parking is integrated within the clusters of apartment parking. Landscaping is sensitively integrated with parking spaces, with planting used to break up rows of parking spaces to create visibly attractive areas. Road layouts are designed in accordance with the Design Manual for Urban Roads and Streets (DMURS, 2019). The basement parking layouts are designed in conjunction with JB Barry Consulting Engineers in compliance with The Design Recommendations for Multi-storey and Underground Car Parks (4th ed. 2011).

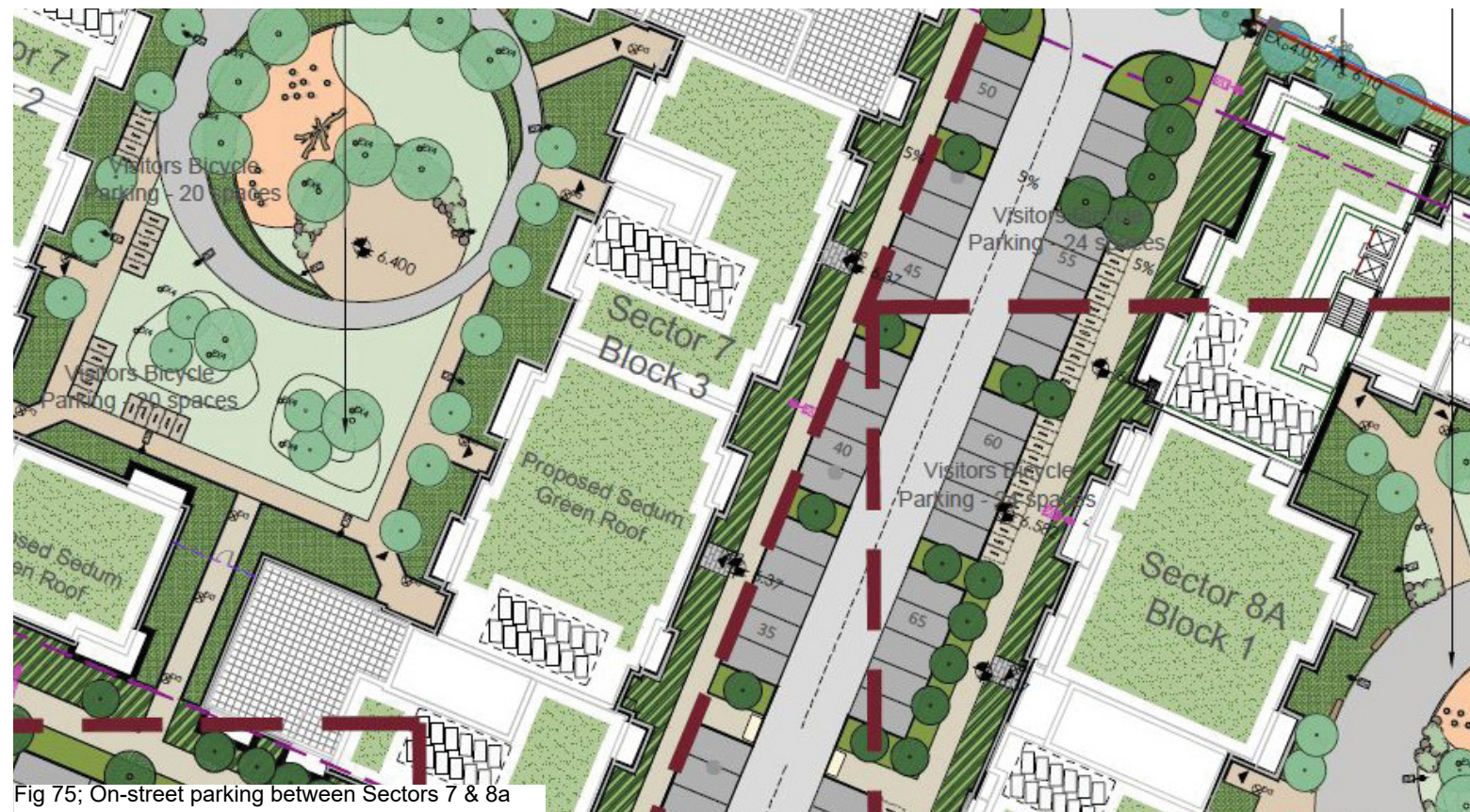
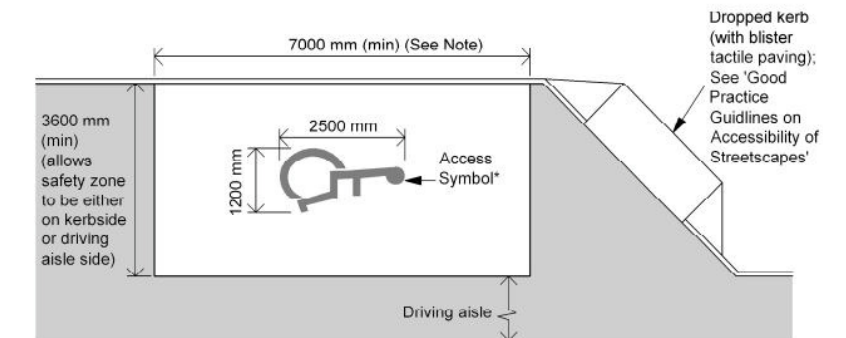
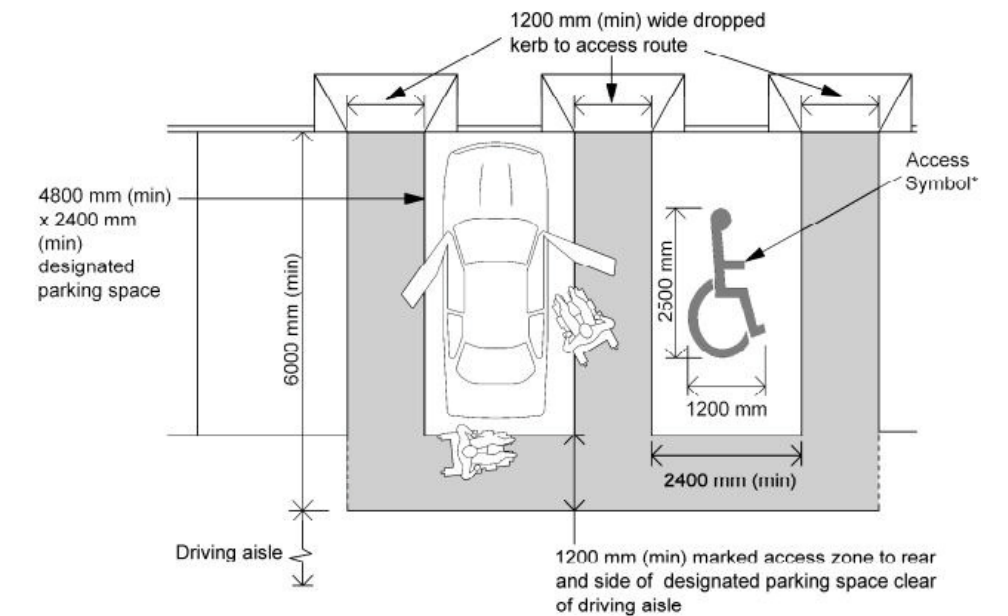


Fig 75; On-street parking between Sectors 7 & 8a



NOTE: Where designated parallel parking bays are provided in series, or in combination with standard parking bays, an additional 2000 mm buffer zone should be provided at the rear of the designated space. The length of the designated parking bay may be reduced to 5800 mm long in this instance.
* Extract from the 'Traffic Signs Manual'

Fig 76;



* Extract from the 'Traffic Signs Manual'

Fig 77;

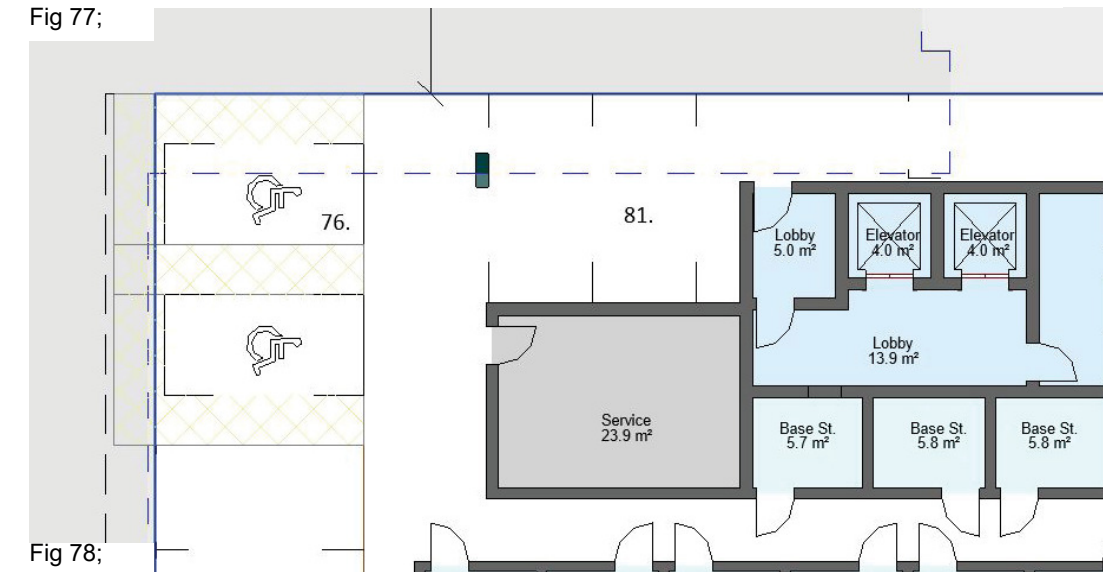


Fig 78;

4.12 Detailed Design

4.12.1 Landscape

The landscape scheme has been designed to create a permeable network of public open spaces linked by a network of streets and pedestrian access links. The design reflects the various distinct character areas, as well as the variety of design throughout the whole scheme.

The sizes of the various communal courtyard gardens is as follows:

- Sector 6a & 6b; 5315sq.m
- Sector 7 4096sq.m
- Sector 8a 913sq.m
- Sector 8b 1530sq.m
- Sector 8c 1083sq.m

Refer to Murray & Associates report for more detail.

4.12.2 Proposed Building Materials

A contemporary palette of materials is proposed, with complimentary tones of brick and render used as the principal finish of all apartment elevations. This is contrasted with a zinc cladding finish to the circulation cores to help break-up the façade. This also helps to differentiate the vertical circulation areas from the residential areas of the buildings, and allows users to acknowledge the stair core entrances.

The use of brick is consistent with the materiality and architectural language of the surrounding area. The use of varieties of buff, red and terracotta-brown brick can be found in the existing adjacent developments at Sycamore House and Red Arches (figs 80 & 81), to the south of our proposed development. Brick in these locations is interspersed with various renders as well as zinc.

While the architectural language of the surrounding area is predominantly brick and render, there is a variety of architectural treatments to be seen such as terracotta and timber cladding, as evident in Clongriffin centre (fig. 79). We have chosen therefor, to enhance the variety of materials in certain areas beyond brick, render and zinc, to those shown.

We have examined the materiality of the adjoining proposed developments at Growth Area 1 (Shoreline 1) to the south and Growth Area 3 (Shoreline 2) to the west (figs 82 & 83). These developments place a strong emphasis on the use of brick panels of varying colour and texture. We have sought to integrate aspects of this architectural language where possible, to bring a consistency to the overall development of all three growth areas 1-3.



Fig 79; Clongriffin village centre



Fig 80



Fig 81

The proposed construction method for the facades is concrete façade panels by Techrete or similar. These can be constructed using various facing textures such as varieties of brick, reconstituted stone or textured concrete. These are intended to be long lasting and durable, and low maintenance, maintaining their colour and appearance in the long term.

The various tones of brick & other material finishes are examined in detail on the following pages. Each of the sectors is differentiated with a variety of different shades of brick and cladding, to emphasize the distinct character areas of each of the sectors. These can be outlined as follows;

- **Sectors 6a & 6b:** A mixture of recon. stone and yellow buff brick, with a zinc cladding to the upper floors
- **Sector 7:** A mixture of dark and light buff brick. Vertical elements such as circulation cores are highlighted in zinc
- **Sector 8a:** An emphasis on recon stone to give a plaster-finish look. The vertical tower element is finished in brick.
- **Sector 8b:** A mixture of recon stone and brick. As in the case of Sector 8a, the vertical tower element is finished in brick to act as a focal point.
- **Sector 8c:** A combination of a textured concrete panel finish, and white recon. stone.

The chosen materials serve to differentiate between the five different sectors proposed as part of the overall scheme. A varied palate of materials, coupled with a varied-but-related architectural language should serve to break-down the ensemble, especially when viewed from Racecourse Park. This, as well as the differing heights to the various buildings give a variety to the elevations to the areas that are visible from the public realm.

The design of the buildings and public spaces will facilitate easy and regular maintenance. Access to the sides of all buildings facilitates efficient maintenance and the chosen palette of materials minimises required maintenance in any case. SVP stacks have been designed to be located internally. Bin stores will be located in basement areas. Particular care has been taken with elevations facing roads and open spaces, to allow for passive surveillance of the public realm.

Landscaping

With regard to materials proposed as part of hard and soft landscaping, please refer to compliance reports and drawings prepared by Landscape architects Murray 7 Associates.



Fig 82; GA3 Proposed development view from Racecourse Park (Image: Henry J Lyons)



Fig 83 GA3 Proposed development (Image: Henry J Lyons Architects)

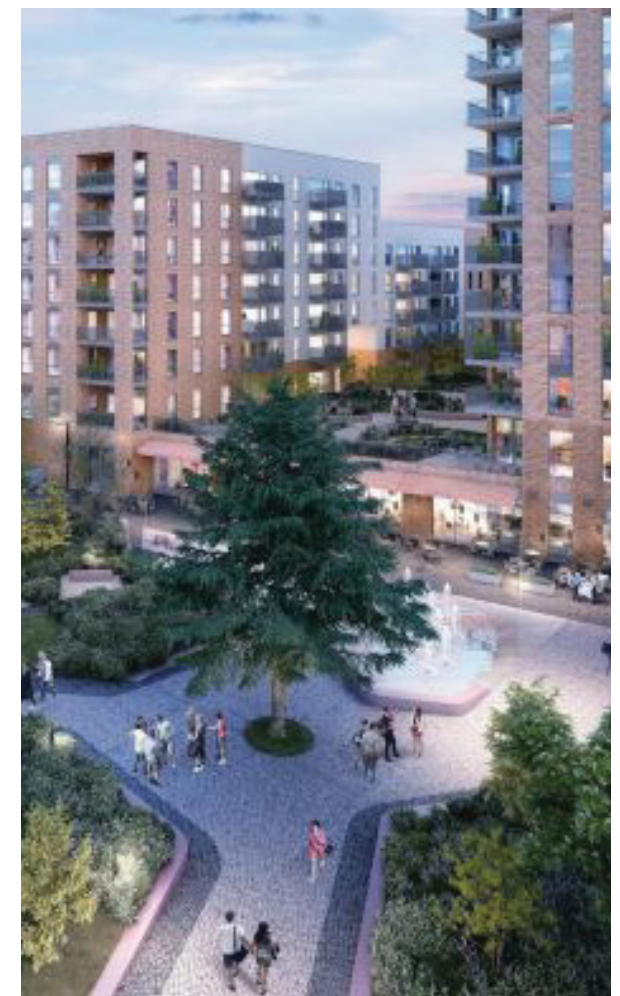




Fig 84; Sector 6a & 6b; Block 1, West Elevation

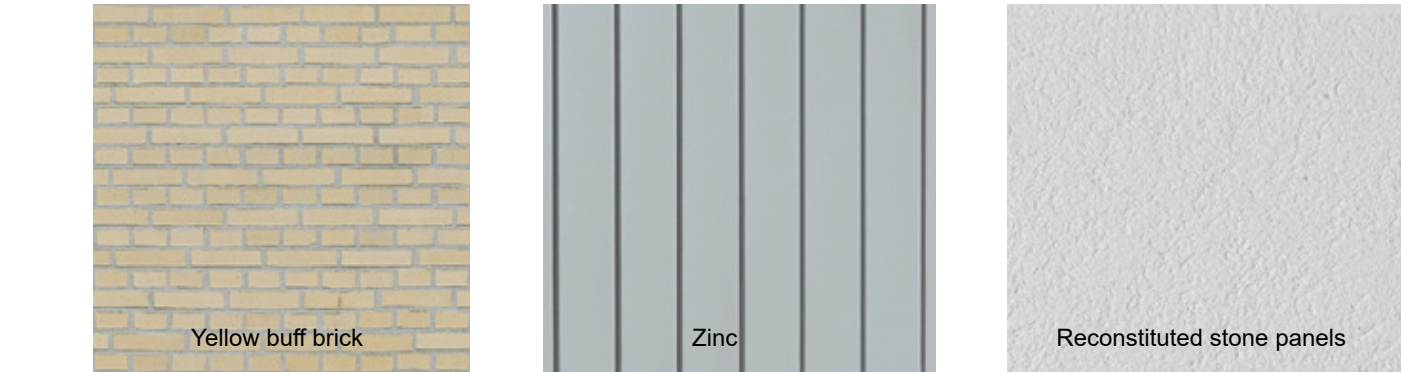


Fig 85; Proposed material samples for Sector 6a & 6b



Fig 86; Sector 6a & 6b; Block 3, East Elevation



Fig 87; Detail panel; Sector 6a & 6b; Block 1, West Elevation



Fig 88; Detail panel; Sector 6a & 6b; Block 1, East Eleva-



Fig 89; Sector 7; Racecourse Park elevation



Fig 90; Detail panel; Sector 7; dark brick with soldier course



Fig 91; Detail panel; Sector 7; light, buff brick with soldier course



Fig 92; Sector 7; Block 1, West Elevation



Fig 93; Detail panel; Sector 7



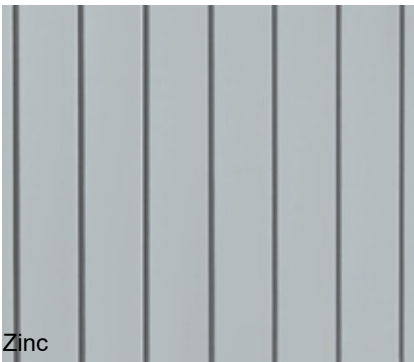
Dark brick



Medium buff brick



Light buff brick



Zinc

Fig 94; Proposed material samples for Sector 7



Fig 95; Sector 8a; Block 1, West Elevation



Fig 96; Detail panel; Sector 8a



Fig 97; Sector 8a; Block 1, South Elevation

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Fig 98; Detail panel; Sector 8a



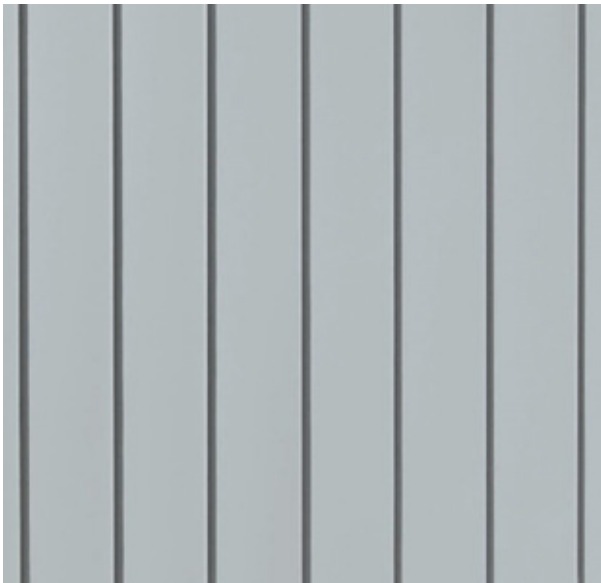
Fig 99; Sector 8a; Block 1 South elevation facing courtyard



Fig 100; Detail panel; Sector 8a



Dark brick, type 1



Zinc



Dark brick, type 2



Reconstituted stone finish

Fig 101; Proposed material samples for Sector 8a



Fig 102; Sector 8b; Block 2 South elevation facing Ireland's Eye avenue



Fig 103; Detail panel, Sector 8b



Fig 104; Sector 8b; South-west elevation facing Stapolin way



Fig 105; Detail panel, Sector 8b



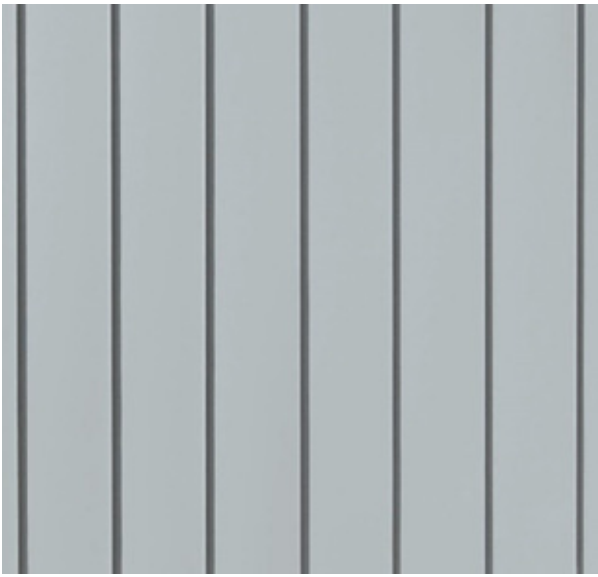
Fig 106; Sector 8b; North-east elevation facong Racecourse park



Fig 107; Detail panel; Sector 8b



Light, buff brick



Zinc



Dark brick



Reconstituted stone finish

Fig 108; Proposed material samples for Sector 8b



Fig 109; Sector 8c; East elevation facing Racecourse park



Fig 110; Detail panel; Sector 8c



Fig 111; Sector 8c; East elevation facing Racecourse park



Texture Concrete finish



Recon stone finish

Fig 112; Proposed material samples for Sector 8c

4.12.3 Waste Collection and Recycling Facilities

There is provision for a three-bin system to be used throughout the scheme. The apartments are provided with secure compactor rooms and Flexible Intermediate Bulk Container (F.I.B.C.) bag storage rooms in the basement areas. Bin stores are to be managed by the Management Company and easily accessible to all apartment residences. Locations and designs of bin stores are to facilitate easy maintenance, without causing a nuisance to adjacent apartments or residences.

The Site Plan illustrates the proposed locations of FIBC bag collection points along the street, for easy refuse collection (figs 113 & 114).



Fig 113; Basement refuse area

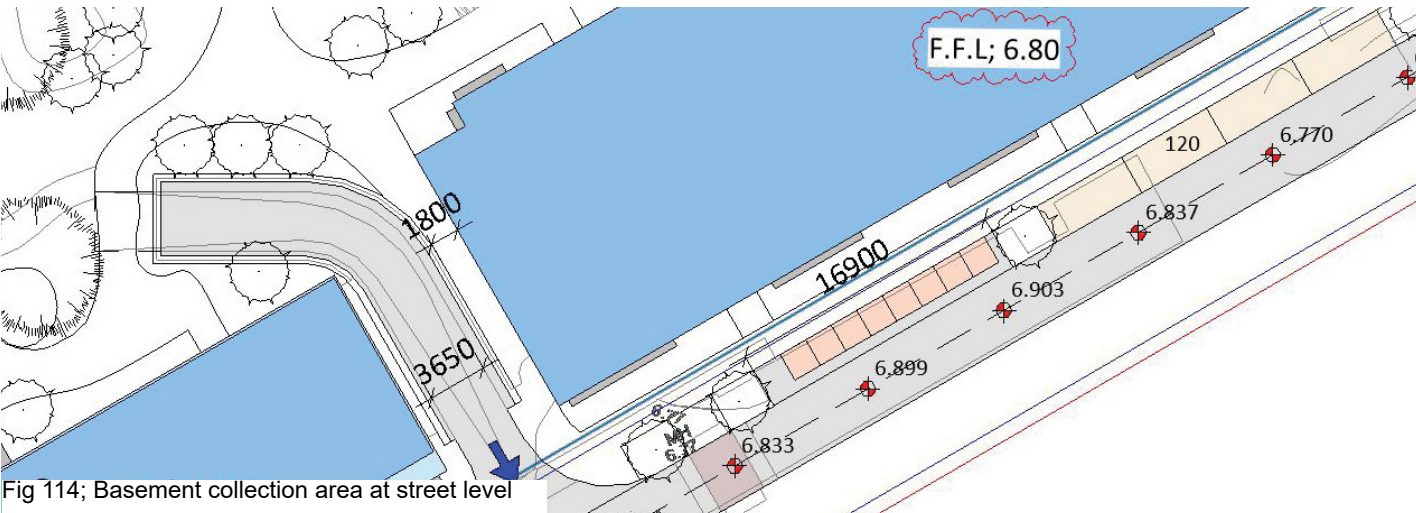


Fig 114; Basement collection area at street level

5.0 Childcare Provision

A purpose built creche (gross floor area of 800sq.m) with a dedicated outdoor play area of c. 208sq.m is proposed at ground floor level of Block 1, Sector 8a. It is shown with dedicated rooms for babies, 1-2 year olds, 2-3 year olds, and 3-6 year olds, various nap areas, kitchen prep area, WC's, male and female lockers, ancillary admin / office area, reception, and entrance lobby. There will be 14no. dedicated creche car parking spaces adjacent to the proposed creche.

Based on the minimum space requirements for a Full Day Care Service or Part-Time Day Care service in the Child Care Act 1991 (Early Years Service) Regulations 2016, the proposed creche could provide up to 161no. child spaces as follows:

- Babies: (120sq.m allocated @ 3.5sq.m per child) = 34 child spaces
- 1-2 yr olds: (85.86sq.m allocated @ 2.8sq.m per child) = 31 child spaces
- 2-3 yr olds: (92.76sq.m allocated @ 2.35sq.m per child) = 39 child spaces
- 3-6 yr olds: (135.3sq.m allocated @ 2.35sq.m per child) = 57 child spaces

If the proposed creche is used for sessional pre-school service or a pre-school service in a drop-in centre, there would be capacity for up to 238 child spaces based on the lower minimum of 1.8 sq.m of clear floor space available for each child attending the service.

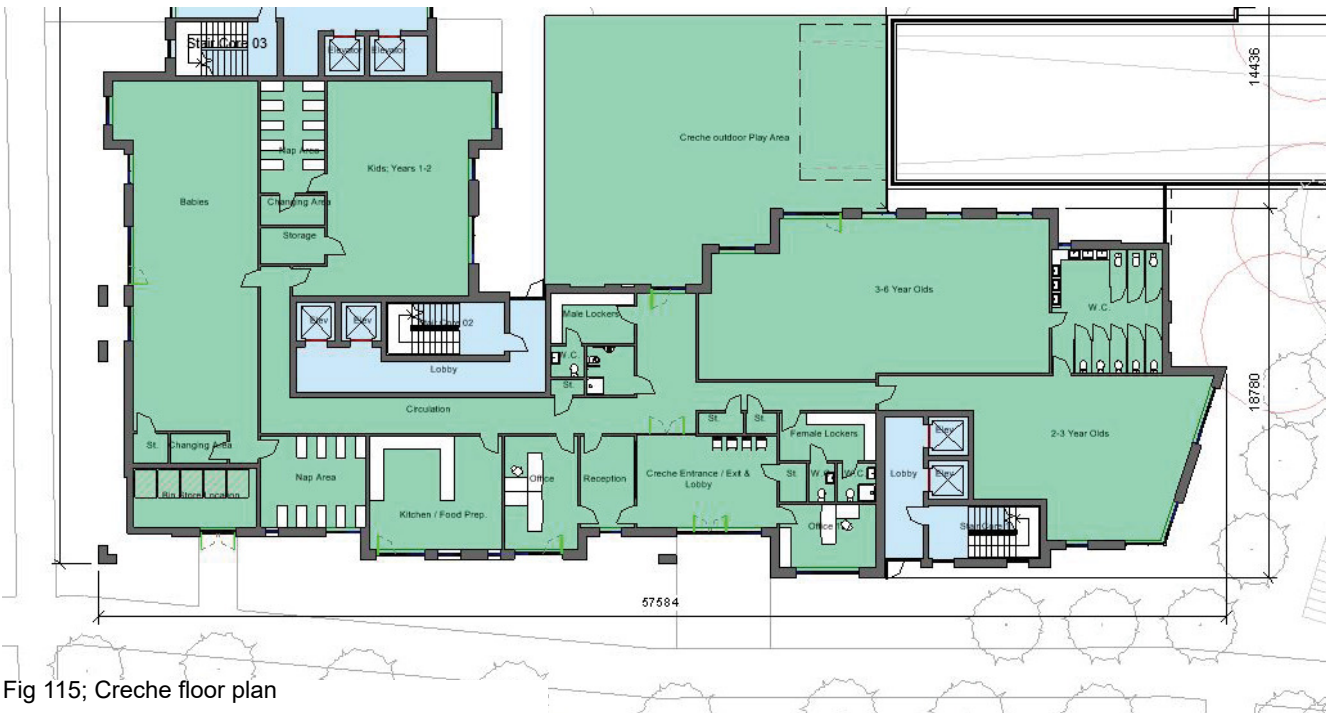


Fig 115; Creche floor plan

6.0 Residential Community Rooms

It is proposed to include some multi-purpose non-residential spaces as part of the scheme. As such, the scheme includes 6no community rooms provided throughout the scheme; (2no in Sector 6a & 6b, 2no in Sector 7, 1no in Sector 8a and 1no in Sector 8b) totalling 515sq.m.

These community rooms are designed to be flexible and could serve a variety of functions such as class rooms, games rooms, remote working hubs or meeting areas for residents.



Fig 116



Fig 117



Fig 118

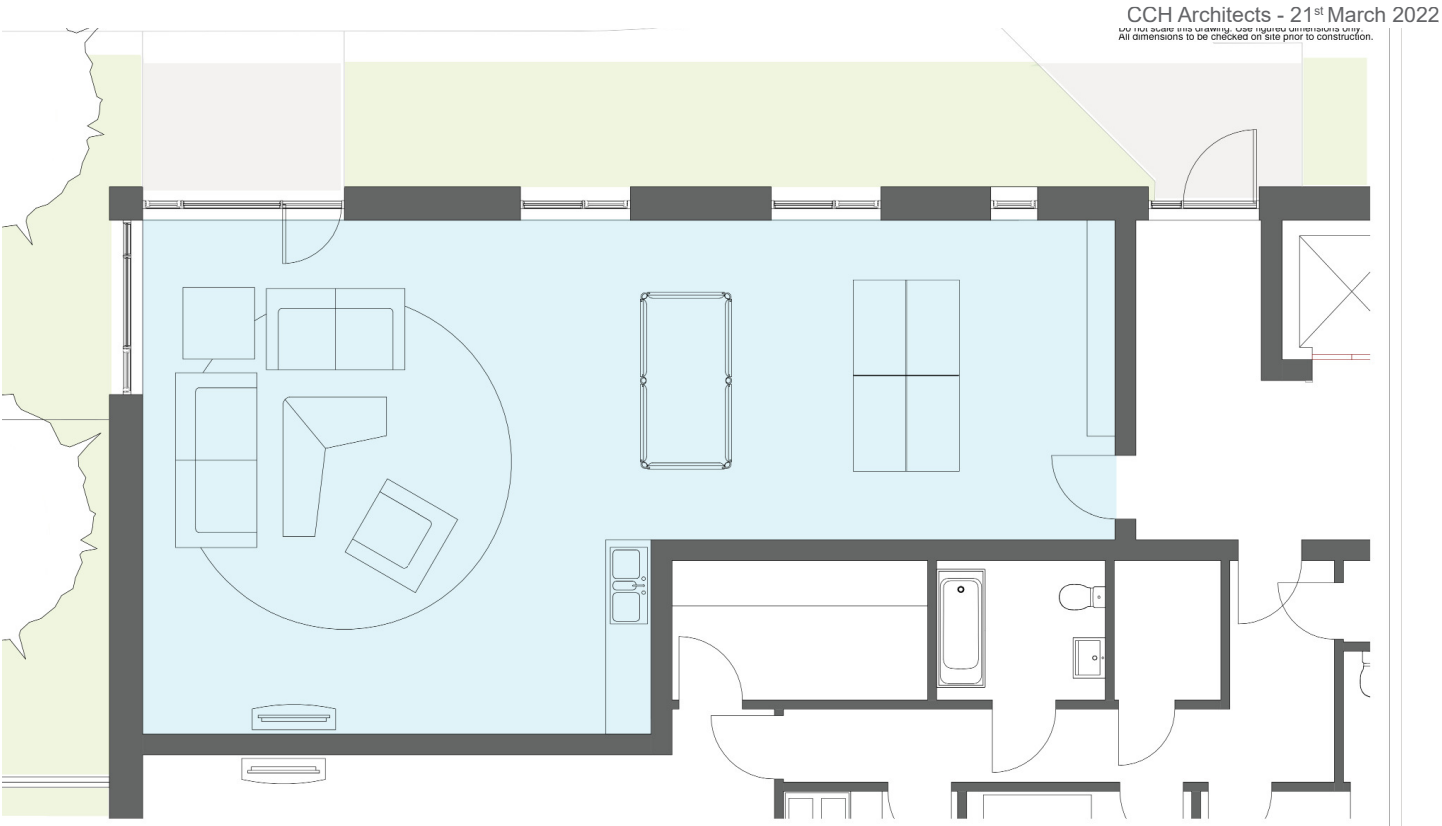


Fig 119; Community room plan with indicative layout

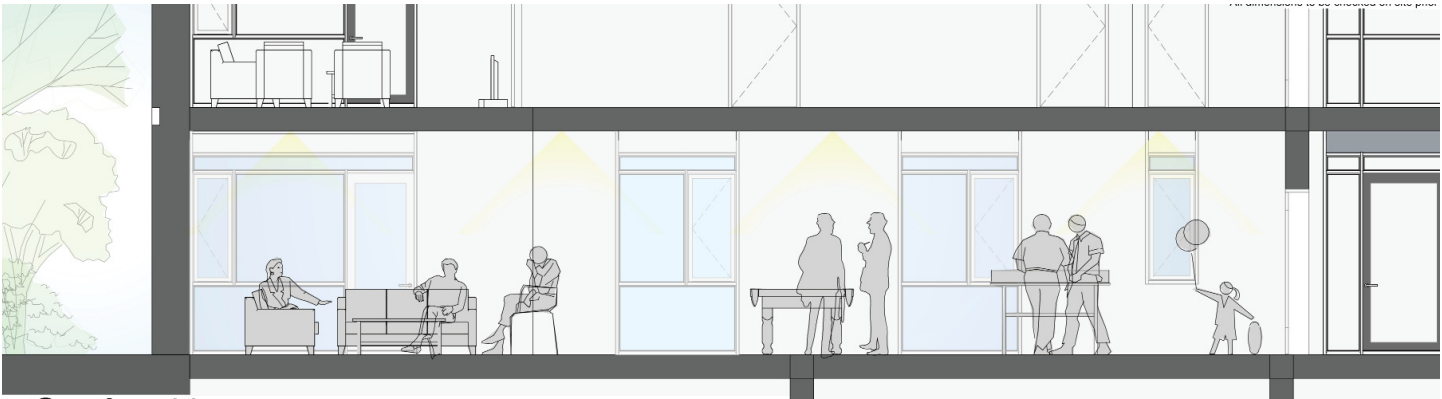


Fig 120; Community room indicative section

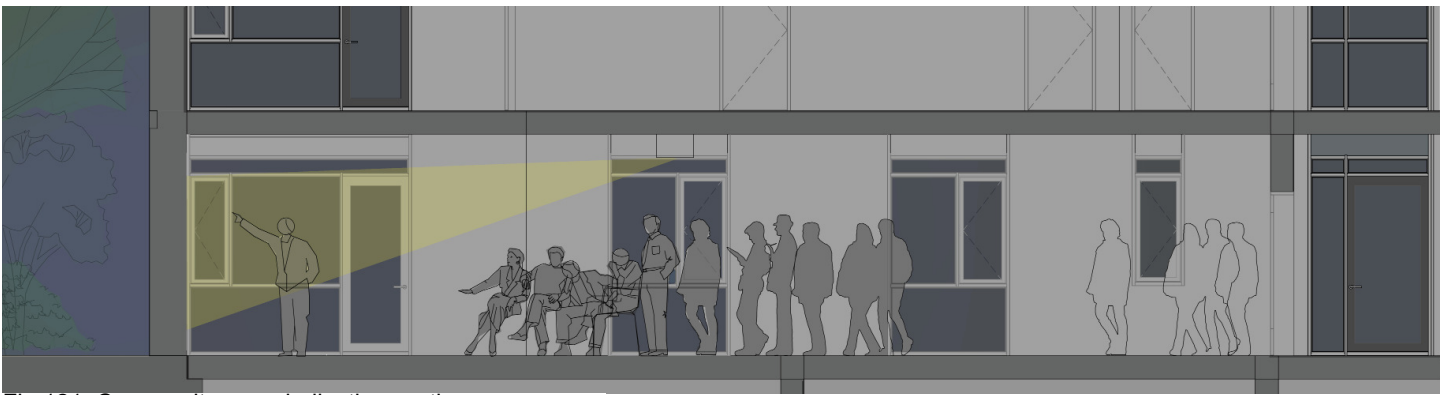


Fig 121; Community room indicative section

7.0 Summary of points raised by An Bord Pleanála, and responses

The following is a summary of points raised in the Inspectors Report, and our responses, following the tripartite meeting between the Design Team, Fingal County Council and An Bord Pleanála in May 2021. This is a non-exhaustive list, primarily dealing with architectural, or related items. ABP comments are shown in *italics*:

7.1 Integration and quality of design

A high architectural input is recommended to the design approach. Improved architectural standard is required and a visual impact on Racecourse Road.

The design has taken account of the following comments with regard to architectural standard, visual Impact and apparent lack of architectural merit.

A principal criticism of the Stage 1 submission was its over-reliance on a single architectural language, minimal palette of materials, and broadly uniform set of building heights.

The key points addressed as part of the re-design are as follows;

- Building heights
- Architectural expression
- Character
- Materials
- Sense of place

Building Heights

For the purpose of uniformity, and in line with development plan objectives, heights along the main streets (Stapolin Way, Stapolin Avenue, Irelands Eye Avenue etc.) have been maintained at a broadly consistent height to give uniformity to the street scape. Node points at the corners of Stapolin Way and Stapolin Avenue have been expressed with slightly higher elements in order to accentuate these corners. The key elevation to Racecourse Park has been developed with a variety of building heights, in order to “break up” this long elevation. Point towers, 8-12 storeys in height are interspersed along this boundary. In terms of height, the proposal ties in with the existing heights of Red Arches (east) and Shoreline 2 (GA3) proposed to the west. The façade is broken up with greater gaps between the buildings, so that the ensemble doesn’t read like a “wall” to Racecourse Park (figs 121 & 122).

Architectural Expression

The scheme seeks to strike a balance between giving a unique character to each of the sectors, while at the same time, bringing a uniformity to the entire scheme. As well as variations in heights, the design introduces various materials such as brick, reconstituted stone, concrete and zinc which are treated in various different ways throughout the different sectors. The design also incorporates a variety of outdoor private spaces to the apartments, such as a variety of balconies and winter gardens.

Character

Each of the various sectors follows a unique character in terms of its form, layout, landscaping and materiality. Variations on the type of brick, architectural expression of window surrounds, balconies, and winter gardens make each sector unique in its own way. Each sector encloses a secure, landscaped courtyard which “set off” the various surrounding facades of the space. Framed views of Racecourse Park also give a special character to the sectors bounding it.



Fig. 122; North & North-East elevation facing Racecourse park; initial proposal, January 2021



Fig. 123; North & North-East elevation facing Racecourse park; revised proposal, January 2022

Park also give a special character to the sectors bounding it.

Materials

Each of the sectors follows a different theme in terms of its architectural expression and materiality. The use of brick, zinc, reconstituted stone panels and concrete panels are used to give a sense of individuality each of the sectors. These are explained in detail in the section “materials” (section 4.12)

Sense of place

As indicated above, each sector has been designed with a variety of materials, heights façade treatments and landscape design to give an individuality and sense of place to each one.

Each sector contains 1-2 courtyard gardens in its centre. Courtyard gardens are designed to be secure to residents, and are generously proportioned to allow ample daylighting. Care has been taken to manage the building heights so as they do not make the courtyard spaces appear oppressive. Each landscaped courtyard has been designed by Murray & Associates landscape architects as usable communal public spaces, with hard and soft landscaping, seating and play areas. Each space is overlooked by the adjoining buildings, and care has been taken to eliminate any blank facades that prevent passive surveillance (section 4.6).

Greater design consideration given to the site bounding the Racecourse Regional Park and the interface. The proposed elevational language of the blocks is repetitious, save only changes in height.

The key issue with the Racecourse Park façade was its uniformity of height and façade expression. This was addressed in the following ways;

- Introduction of higher elements / towers to give variety to the roofline
- Greater gaps between buildings to break up the regularity / uniformity of this façade
- Introduction of a varied palette of materials and façade treatments.
- Introduction of a variety of balconies, winter gardens and window treatments.

The proposed proportions of, and graduations to the 9 storey high elements could be further refined to great visual prominence to higher vertical elements and great visual interest. The architectural expression and treatment of higher elements could be developed to act as a contrast and centre point within the urban blocks. There should be a range of materials and finishes. A materials strategy shall include details of the colour, tone and texture of materials and the modelling and profile of the materials on each block.

The introduction of towers to the Racecourse Park side of the scheme helps to give variety to this long elevation. Furthermore, as in the case of Sectors 8a & 8b, the towers have been expressed in brick to contrast with some of the surrounding recon stone. These towers provide a focus from both the internal courtyard and the large park beyond.



Fig. 124; Sector 6a; Courtyard at dusk.



Fig. 125; Sector 8c from Stapolin Haggard

There should be a main focal point for the entrances

Communal entrances to apartment buildings are articulated in a variety of ways:

- Different expression in terms of materiality
- Increased height at roof level, to give variety to the roof scape.
- Different window treatments

The urban design approach should be further considered in the context of resulting opposing apartment windows and privacy.

The positioning of directly opposing windows to habitable rooms at first floor and over has been designed to a minimum 22m in line with housing standards. Glazing to non-habitable rooms (bathrooms, utility etc.) can be designed with opaque glazing, where privacy may become an issue. For further information, please refer to Section 5.6; "Distinctiveness" as well as the architects floor plan drawings.

In line with apartment standards, there is a buffer maintained between public footpaths and ground floor windows of a minimum 1.5m. In almost all cases, this distance is increased. Low planting and shrubbery, as shown on the landscape drawings, further enhance this privacy strip. Refer to Section 5.9.2 "Universal Access" which illustrates how these privacy strips work in practice (image fig. 71).

Detailing of the roof spaces

Roof spaces are predominantly to be used as Sedum roofs, or "green roofs". These are shown in detail on the axonometric drawings and landscape roof drawings. Apartment roof areas have been allocated to solar panels as part of the sustainability strategy for the scheme. Roof terraces have been introduced to Sector 7. These are indicated on the drawings, to serve as communal terraces for residents, with an opportunity to take in the views of Racecourse Park, and Ireland's eye beyond (figs 126 & 127).

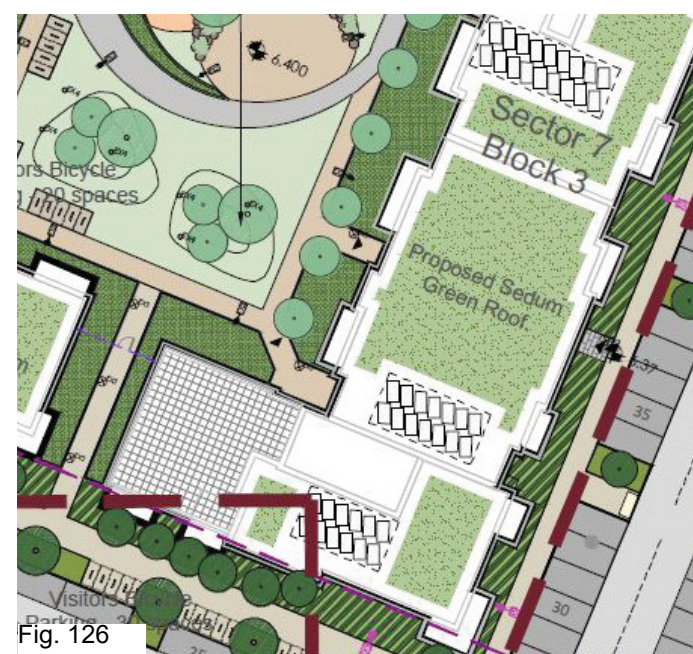


Fig. 126



Fig. 127



Fig. 128; Sector 8a, courtyard view looking north

7.2 Residential Amenity and Unit Layout

The Housing Quality Assessment should include a schedule of floor areas, clearly set out floor areas, aspect and justification for each unit

The Housing Quality Statements for each of the 1007no apartment units indicates which of the unit floor areas are greater than 10% of the minimum floor area stipulated in the Design Standards for New Apartments 2020. These calculations are contained in the following documents:

- BALN5-CCH-00-ZZ-REP-A-230 Sector 6a & 6b Housing Quality Statement
- BALN5-CCH-00-ZZ-REP-A-231 Sector 7 Housing Quality Statement
- BALN5-CCH-00-ZZ-REP-A-232 Sector 8a Housing Quality Statement
- BALN5-CCH-00-ZZ-REP-A-233 Sector 8b Housing Quality Statement
- BALN5-CCH-00-ZZ-REP-A-234 Sector 8c Housing Quality Statement

In addition, the Apartment Types study for each of the sectors profiles each apartment type outlining the key areas and dimensions (minimum allowable under guidelines, as well as proposed). These are contained in the following documents:

- BALN5-CCH-00-ZZ-REP-A-241 Apartment Types Sector 6a & 6b
- BALN5-CCH-00-ZZ-REP-A-242 Apartment Types Sector 7
- BALN5-CCH-00-ZZ-REP-A-243 Apartment Types Sector 8a
- BALN5-CCH-00-ZZ-REP-A-244 Apartment Types Sector 8b
- BALN5-CCH-00-ZZ-REP-A-245 Apartment Types Sector 8c

The proposal includes 15% bed three-person units. The guidelines state that only 10% of these units are permitted. This is in contravention of national policy.

The design has now been amended to show 9.3% of the total number of units proposed as 2-bed three-person units. This is below the threshold of 10% as stated in national policy guidelines

There is no parking for the crèche which requires 15no spaces

The site plan has been amended to indicate crèche parking to the south and west of the crèche, including set-down spaces.



Fig. 129; Sector 8b, courtyard view



Fig. 130; Sector 8c, courtyard view from Racecourse Park

7.3 Transport Planning

Bicycle parking to be provided within national standards

In accordance with the Design Standards for New Apartments (ed.2020), bicycle parking is provided at the following ratio;

- 1no bicycle space per bedroom space (1754no spaces)
- 1no visitor bicycle space per 2no units (500no spaces)
- In addition, there are 21no spaces allocated to the crèche. For staff.

Design Standards for New Apartments 2020 state that cycle storage facilities shall be provided in a dedicated facility of permanent construction, or within an adjacent or adjoining purpose-built structure of permanent construction. Residents bicycle parking is provided in dedicated storage rooms or within secure courtyards. Visitor bicycle storage areas are directly assessible from the public road, located in areas with passive surveillance

Basement parking should be in line with national standards

The total number of basement parking spaces is as follows:

Sector 6:	190no spaces (incl. 10no disabled)
Sectors 7&8a:	305no spaces (incl. 16no disabled)
Sectors 8b&8c:	110no spaces (incl. 6no disabled)
Total:	605no basement parking spaces (incl 32 no disabled)

Basement parking has been designed with reference to the Design Recommendations for Multi-Sto-
rey and Underground Car Parks (4th ed.) 2011)

A swept path analysis for basement parking should be provided

A swept path analysis has been provided by JB Barry & Associates engineers, (20211-JBB-00-XX-
DR-C-01009) and accompanies the design documentation.

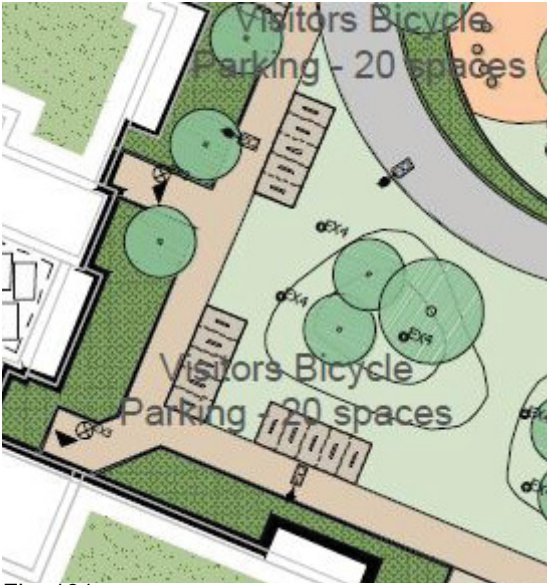
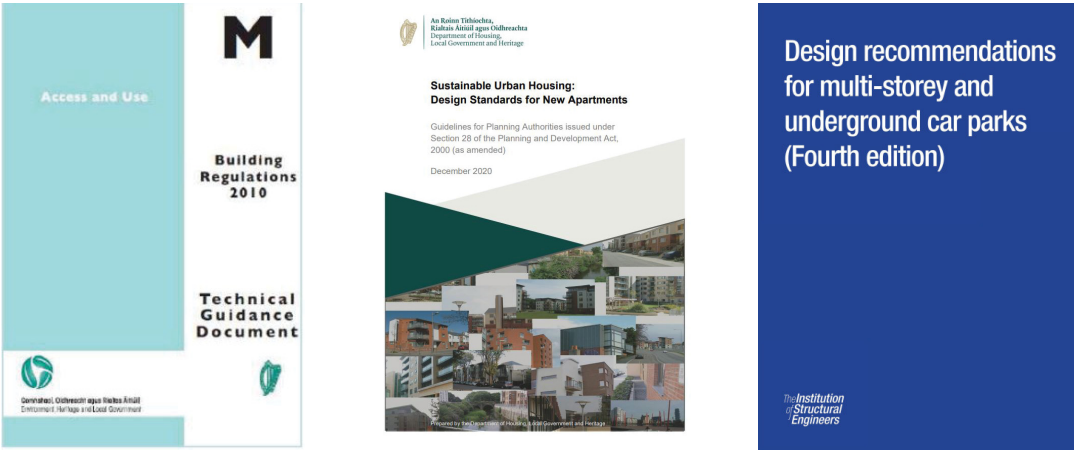
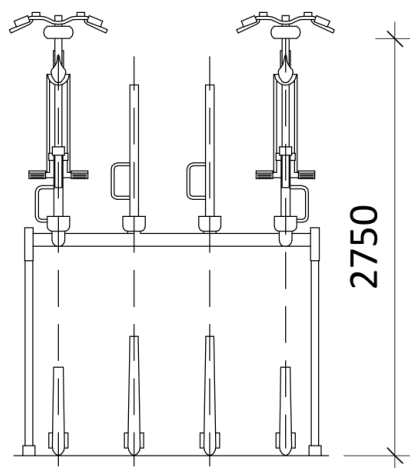
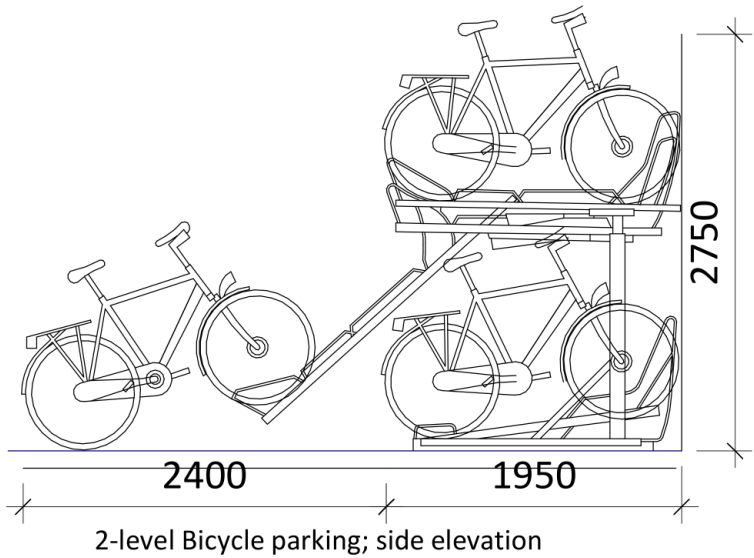


Fig. 131

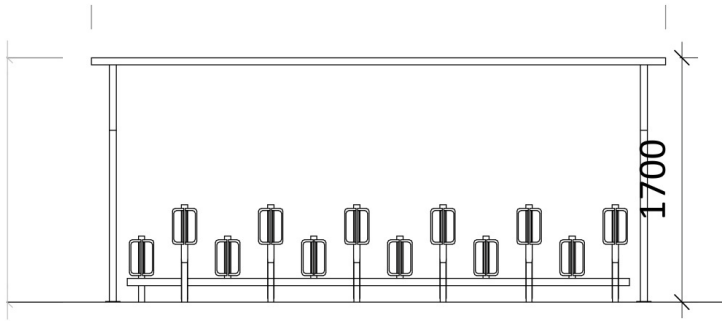


Fig. 132

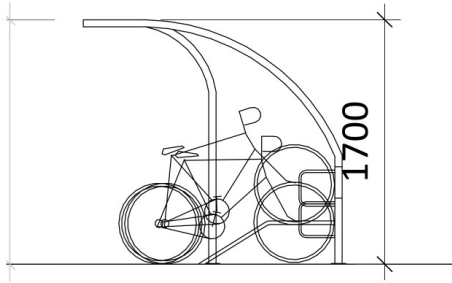


2-level Bicycle parking; front elevation

2-level Bicycle parking; side elevation



Typical Bicycle parking (12 bays) front elevation



Side elevation

Fig. 133

8.0 Perspective views and CGI Images



Fig. 134; View of the proposed development from Stapolin Haggard



Fig. 135; View of the proposed development from Stapolin Haggard
BALN5-CCH-00-ZZ-REP-A-240 P04 DESIGN STATEMENT INCLUDING 12 POINT DESIGN APPRAISAL



Fig. 136; View of the proposed development from Racecourse Park



Fig. 137; View of "Irelands Eye Avenue" from Racecourse Park



Fig. 138; Aerial view of "Stapolin Way" looking south-east



Fig. 139; Aerial view of "Stapolin Way" looking east

ARTHUR HICKEY